



PLANNING AND ZONING COMMISSION

CITY OF CORINTH, TEXAS

3300 CORINTH PARKWAY, CORINTH, TEXAS

SPECIAL SESSION MEETING

MONDAY, APRIL 18, 2016 – 7:00 P.M.



*** * * * PUBLIC NOTICE * * * ***

**NOTICE OF THE CITY OF CORINTH
PLANNING AND ZONING COMMISSION SPECIAL SESSION
MONDAY, APRIL 18, 2016 7:00 P.M.
CITY HALL – 3300 CORINTH PARKWAY**

CALL TO ORDER, INVOCATION

CONSENT AGENDA

All matters listed under the Consent Agenda are considered to be routine and will be enacted in one motion. Should the Chairman, Commissioner, or any citizen desire discussion of any Item that Item will be removed from the Consent Agenda and will be considered separately.

1. Consider and act on Minutes from the March 28, 2016 Planning and Zoning Commission Regular Session.

BUSINESS AGENDA

2. Consider and act on the Preliminary Plat of Terrace Oaks Addition Phase I, Lots 1-33, Block 1; Lots 1-30, Block 2; Lots 1-16, Block 3; Lots 1-13, Block 4; Lots 1-16, Block 5, being 31.364 acres situated in the W.M. Wilson Survey, County Abstract 1383, the D.A. Ware Survey, County Abstract 1580, and the W.M. Garrison Survey, County Abstract 0508 in the City of Corinth, Denton County, Texas.
3. Consider and act on the Final Plat of Terrace Oaks Addition Phase I, Lots 1-33, Block 1; Lots 1-30, Block 2; Lots 1-16, Block 3; Lots 1-13, Block 4; Lots 1-16, Block 5, being 31.364 acres situated in the W.M. Wilson Survey, County Abstract 1383, the D.A. Ware Survey, County Abstract 1580, and the W.M. Garrison Survey, County Abstract 0508 in the City of Corinth, Denton County, Texas.

PUBLIC HEARING / BUSINESS AGENDA ITEM

4. TO HEAR PUBLIC OPINION REGARDING A REQUEST FROM THE APPLICANT DALE RIDINGER, WITH RIDINGER ASSOCIATES, INC, AUTHORIZED REPRESENTATIVE FOR THE PROPERTY OWNERS, JOHN HOFFLER AND SUSAN TORRIE FOR A ZONING CHANGE FROM SINGLE FAMILY-2 (SF-2) TO PLANNED DEVELOPMENT SINGLE FAMILY-3 PROVIDING A CONCEPT DESIGN MAP ON A TOTAL OF 2.658 ACRES LEGALLY DESCRIBED AS A PORTION OF J. HOFFLER ADDITION, BEING A PART OF A CERTAIN TRACT OF LAND DESCRIBED BY DEED TO JOHN V. HOFFLER RECORDED IN VOLUME 2853, PAGE 707 OF THE PRDCT AND BEING TRACT 3 SITUATED IN THE J.M. WALTON SURVEY, COUNTY ABSTRACT 1390 IN THE CITY OF CORINTH, DENTON

COUNTY, TEXAS. (This property is located on the south side of Fritz Lane - 3618 Fritz Lane).

- 4a. Consider and act on a zoning change from Single Family-2 (SF-2) to Planned Development Single Family-3 on a total of 2.658 acres legally described as a portion of J. Hoeffler Addition, and being a part of a certain tract of land described by deed to John V. Hoeffler recorded in Volume 2853, Page 7-7 of the PRDCT and being Tract 3 situated in the J.M. Walton Survey, County Abstract 1390 in the City of Corinth, Denton County, Texas.

BUSINESS AGENDA

5. Consider and act on a request from the applicant, Dale Ridinger with Ridinger Associates Inc., authorized representative for the property owner, for a Major Waiver to City of Corinth Ordinance No. 13-05-08-20, Unified Development Code (UDC) to allow an 80 foot turnaround outside paving diameter in lieu of the 100 foot outside paving diameter as required in the UDC; and an 81 foot diameter temporary turnaround easement in lieu of the 120 feet dimension as required for the Right-of-Way in the UDC on a total of 2.658 acres legally described as a portion of J. Hoeffler Addition, and being a part of a certain tract of land described by deed to John V. Hoeffler recorded in Volume 2853, Page 7-7 of the PRDCT and being Tract 3 situated in the J.M. Walton Survey, County Abstract 1390 in the City of Corinth, Denton County, Texas.
6. Consider and act on the Preliminary Plat of Parkside Farms Phase II Lots 1-4, Block E and Lots 1-4, Block F being 2.658 acres situated in the J.M. Walton Survey, County Abstract 1390 including a portion of J. Hoeffler Addition recorded in Cabinet L, Page 46 PRDCT in the City of Corinth, Denton County, Texas.

PUBLIC HEARING / BUSINESS AGENDA ITEM

7. TO HEAR PUBLIC OPINION REGARDING A FINAL PLAT OF PARKSIDE FARMS PHASE II LOTS 1-4, BLOCK E AND LOTS 1-4, BLOCK F BEING 2.658 ACRES SITUATED IN THE J.M. WALTON SURVEY, COUNTY ABSTRACT 1390 INCLUDING A REPLAT OF A PORTION OF J. HOFFFLER ADDITION RECORDED IN CABINET L, PAGE 46 PRDCT IN THE CITY OF CORINTH, DENTON COUNTY, TEXAS. (his property is located at 3618 Fritz Lane (south side of Fritz and at the end of Belle Way.)
- 7a. Consider and act on the Final Plat of Parkside Farms Phase II Lots 1-4, Block E and Lots 1-4, Block F being 2.658 acres situated in the J.M. Walton Survey, County Abstract 1390 including a Replat of a portion of J. Hoeffler Addition recorded in Cabinet L, Page 46 PRDCT in the City of Corinth, Denton County, Texas.

BUSINESS AGENDA

8. Consider and act on a request from the applicant Steve Homeyer, authorized representative for the property owner, Redriver Bancorp for a major subdivision waiver to the City of Corinth Ordinance No. 13-05-08-20, Unified Development Code (UDC) to allow a reduction in the minimum required distance from the intersection along a collector to the centerline of a proposed driveway on property legally described as part of Tract 10A being

2.21 acres out of the J.P. Walton Survey, Abstract No. 1389, in the City of Corinth, Denton County, Texas.

PUBLIC HEARING / BUSINESS AGENDA ITEM

9. TO HEAR PUBLIC OPINION REGARDING A REQUEST FROM THE APPLICANT, KYLE VRLA, AUTHORIZED REPRESENTATIVE FOR THE PROPERTY OWNER, MONICA UTTER, WITH UTTER PROPERTIES, FOR A ZONING CHANGE ON 12.283 ACRES FROM C-3, COMMERCIAL TO PLANNED DEVELOPMENT C-3 ON PROPERTY LEGALLY DESCRIBED AS BILL UTTER FORD ADDITION BLOCK A, LOT 1 AND A ZONING CHANGE ON APPROXIMATELY 6.255 ACRES FROM C-2, COMMERCIAL TO PLANNED DEVELOPMENT C-2 ON PROPERTY LEGALLY DESCRIBED AS BILL UTTER FORD ADDITION BLOCK A, LOT 2 AND TRACT 26 OUT OF THE L. BATES SURVEY, COUNTY ABSTRACT 204 IN THE CITY OF CORINTH, DENTON COUNTY, TEXAS.
- 9a. Consider and act on a zoning change on 12.283 acres from C-3, Commercial to Planned Development C-3 on property legally described as Bill Utter Ford Addition Block A, Lot 1 AND a zoning change on approximately 6.255 acres from C-2, Commercial to Planned Development C-2 on property legally described as Bill Utter Ford Addition Block A, Lot 2 and Tract 26 out of the L. Bates Survey, County Abstract 204 in the City of Corinth, Denton County, Texas.

PUBLIC HEARING / BUSINESS AGENDA ITEM

10. TO HEAR PUBLIC OPINION REGARDING A REQUEST FROM THE APPLICANT JOHN THATCHER, AUTHORIZED REPRESENTATIVE FOR THE PROPERTY OWNERS, TOMMY JOSEPH ROBINSON, BRIAN KEITH ROBINSON, RAYMON MANN ROBINSON, BRANDY DENISE ROBINSON, ROBERT MANLEY ROBINSON, THOMAS MANN ROBINSON, AND TIMOTHY MANLEY ROBINSON FOR A ZONING CHANGE FROM C-1, COMMERCIAL TO PLANNED DEVELOPMENT C-2 COMMERCIAL WITH MODIFIED DEVELOPMENT STANDARDS ON 10.603 ACRES OF LAND LEGALLY DESCRIBED AS LOT 1, BLOCK A, TRI-STEEL ADDITION, IN THE CITY OF CORINTH, DENTON COUNTY, TEXAS.
- 10a. Consider and act on a zoning change from C-1, Commercial to Planned Development C-2 on 10.603 acres of land legally described as Lot 1, Block A, Tri-Steel Addition, in the City of Corinth, Denton County, Texas. (This property is located on the south side of I-35E, between Post Oak Drive and Church Drive.)

PUBLIC HEARING / BUSINESS AGENDA ITEM

11. TO HEAR PUBLIC OPINION REGARDING A REQUEST FROM THE APPLICANT RAY HUFFINES AUTHORIZED REPRESENTATIVE FOR DWIGHT WALKER, MANAGING GENERAL PARTNER FOR THE PROPERTY OWNERS DOROTHY WILLIAMS FAMILY PS LTD AND R. O. DUNCAN FOR AN AMENDMENT TO THE COMPREHENSIVE PLAN FUTURE LAND USE DESIGNATION FROM MIXED USE NON-RESIDENTIAL TO COMMERCIAL ON 13.773 ACRES OF LAND SITUATED IN

THE E. A. GARRISON SURVEY, COUNTY ABSTRACT 511 BEING TRACTS 36, 35, 34, 33, AND 24 IN THE CITY OF CORINTH, DENTON COUNTY, TEXAS.

- 11a. Consider and act on an amendment to the Comprehensive Plan Future Land Use designation from Mixed Use Non-Residential to Commercial on 13.773 acres of land situated in the E. A. Garrison Survey, County Abstract 511 being tracts 36, 35, 34, 33, and 24 in the City of Corinth, Denton County, Texas.

PUBLIC HEARING / BUSINESS AGENDA ITEM

12. TO HEAR PUBLIC OPINION REGARDING A REQUEST FROM THE APPLICANT RAY HUFFINES AUTHORIZED REPRESENTATIVE FOR DWIGHT WALKER, MANAGING GENERAL PARTNER FOR THE PROPERTY OWNERS DOROTHY WILLIAMS FAMILY PS LTD AND R. O. DUNCAN FOR A ZONING CHANGE FROM MX-C, MIXED USE COMMERCIAL TO PLANNED DEVELOPMENT MX-C ON 13.773 ACRES OF LAND SITUATED IN THE E. A. GARRISON SURVEY, COUNTY ABSTRACT 511 BEING TRACTS 36, 35, 34, 33, AND 24 IN THE CITY OF CORINTH, DENTON COUNTY, TEXAS.

- 12a. Consider and act on a zoning change from MX-C, Commercial to Planned Development MX-C on 13.773 acres of land situated in the E. A. Garrison Survey, County Abstract 511 being tracts 36, 35, 34, 33, and 24 in the City of Corinth, Denton County, Texas.

EXECUTIVE SESSION

As authorized by Section 551.071a(2) of the Texas Government Code, this meeting may be convened into closed session for the purpose of seeking confidential legal advice of the City Attorney on any Agenda Item listed herein.

As a majority of Council Members of the City of Corinth may attend the above described meeting, this notice is given in accordance with Chapter 551 of the Texas Government Code. No official action will be taken by the City Council at this meeting.

ADJOURN REGULAR SESSION

Posted this 12th day of April, 2016 before 5:00 p.m. on the bulletin board at Corinth City Hall.



Nathan Abato
Planning and Zoning Commission
Planning and Development Coordinator
City of Corinth, Texas.

Corinth City Hall is wheelchair accessible and accessible parking spaces are available. Requests for accommodations or interpretive services must be made 48 hours prior to this meeting. Please contact the City Secretary's office at 940.498-3200 or FAX 940.498-7505 for more information.

BRAILLE IS NOT AVAILABLE

CONSENT AGENDA ITEM #1

Planning and Zoning Commission Special Session

April 18, 2016

CONSENT AGENDA ITEM

1. Consider and act on Minutes from the March 28, 2016 Planning and Zoning Commission Regular Session Meeting.

AGENDA ITEM SUMMARY/BACKGROUND

Consideration of the minutes from the March 28, 2016 Planning and Zoning Regular Session Meeting.

INITIAL SUMMARY

to not of funding

RECOMMENDATION

ATTACHMENTS / SUPPORTING DOCUMENTS

Copy of the minutes.

Submitted By: Nathan Abato, Planning and Development Department

Finance Review: NA

Legal Review: NA

**STATE OF TEXAS
COUNTY OF DENTON
CITY OF CORINTH**

On this March 28, 2016, The Planning and Zoning Commission of the City of Corinth, Texas, met in a Regular Session at Corinth City Hall, located at 3300 Corinth Parkway, Corinth, TX. The meeting date, time, place and purpose as required by Title 5, Subtitle A, Chapter 551, Subchapter C, Section 551.041, Government Code, with the following members, to wit:

Members Present:

Brian Rush
Bruce Hanson
Dwayne Zinn
Breien Velde
Haven Hendrik

Members Absent:

Bill Morgan
Marc Powell

CALL TO ORDER

The Regular Session Meeting of the Planning & Zoning Commission of the City of Corinth, Texas, was called to order by Brian Rush at 7:00 p.m.

INVOCATION

Mr. Zinn delivered the invocation.

CONSENT AGENDA

1. Consider and act on Minutes from the February 22, 2016 Planning and Zoning Commission Regular Session Schedule.

MOTION made by Mr. Hanson to approve the consent agenda.

SECONDED by Mr. Zinn.

5-0. All in favor.

Ayes: Haven Hendrik, Brian Rush, Dwayne Zinn, Bruce Hanson, Breien Velde
Noes: none
Absent: Bill Morgan, Marc Powell

MOTION TO APPROVE CARRIES

2. Consider and act on the Preliminary Plat of Terrace Oaks Addition Phase I, Lots 1-33, Block 1; Lots 1-30, Block 2; Lots 1-16, Block 3; Lots 1-13, Block 4; Lots 1-16, Block 5, being 31.364 acres situated in the W.M. Wilson survey, County Abstract 1383, being tracts 18, 19, 20, 21, 22, 23 and 25; situated in the D.A. Ware survey, County Abstract 1580, being tract 3; situated in the W.M. Garrison survey, county abstract 0508, being tract 10; in the City of Corinth, Denton County, Texas. (This site is located on the east side of the 2100, 2200, and 2300 blocks of Post Oak Drive.)

Ms. Cabbage informed the commission that the applicant has submitted a written request to table this business item until the next scheduled Planning and Zoning Commission meeting.

BUSINESS ITEM TABLED UNTIL NEXT MEETING

3. Consider and act on the Final Plat of Terrace Oaks Addition Phase I, Lots 1-33, Block 1; Lots 1-30, Block 2; Lots 1-16, Block 3; Lots 1-13, Block 4; Lots 1-16, Block 5, being 31.364 acres situated in the W.M. Wilson survey, County Abstract 1383, being tracts 18, 19, 20, 21, 22, 23 and 25; situated in the D.A. Ware survey, County Abstract 1580, being tract 3; situated in the W.M. Garrison survey, county abstract 0508, being tract 10; in the City of Corinth, Denton County, Texas. (This site is located on the east side of the 2100, 2200, and 2300 blocks of Post Oak Drive.)

Ms. Cabbage informed the commission that the applicant has submitted a written request to table this business item until the next scheduled Planning and Zoning Commission meeting.

BUSINESS ITEM TABLED UNTIL NEXT MEETING

4. TO HEAR PUBLIC OPINION REGARDING A REQUEST BY THE APPLICANT SOUTH DENTON SELF STORAGE FOR A VARIANCE TO THE UDC (UNIFIED DEVELOPMENT CODE) 4.02.08 GENERAL FENCE REQUIREMENTS D CONSTRUCTION MATERIAL, TO ALLOW A CHAIN LINK AS THE CONSTRUCTION MATERIAL ON PROPERTY LEGALLY DESCRIBED AS YE OLDE LAND COMPANY ADDITION NO. 1 HAVING A PHYSICAL ADDRESS OF 6501 IH 35E IN THE CITY OF CORINTH, DENTON COUNTY, TEXAS.

PUBLIC HEARING opened at 7:08 PM.

PUBLIC HEARING closed at 7:08 PM.

- 4a. Consider and act on a request for a variance to the UDC (Unified Development Code) 4.02.08 General Fence Requirements D Construction Material, to allow a chain link as the construction material on property legally described as Ye Olde Land Company Addition No. 1 having a physical address of 6501 IH 35E in the City of Corinth, Denton County, Texas.

Ms. Cabbage gave a brief overview of the fence area where the work was done without a permit. She also said that had the fence contractor applied for a fence permit, he would have been informed of the fence's non-conforming use status.

Mr. Hanson asked if the rest of the fence line is made of chain link. Ms. Cabbage answered yes.

MOTION to approve the variance made by MR. Hanson.

SECONDED by Ms. Hendrik.

Ayes: Haven Hendrik, Brian Rush, Dwayne Zinn, Bruce Hanson, Breien Velde
 Noes: none
 Absent: Bill Morgan, Marc Powell

MOTION TO APPROVE CARRIES

ADJOURN

Meeting adjourned at 7:11 PM.

MINUTES APPROVED THIS _____ DAY OF _____, 2016.

Brian Rush, Planning and Zoning Commission Chairman

Nathan Abato, Planning and Zoning Commission Secretary

BUSINESS AGENDA ITEM # 2

Planning and Zoning Commission Special Session

April 18, 2016

AGENDA ITEM

2. Consider and act on the Preliminary Plat of Terrace Oaks Addition Phase I, Lots 1-33, Block 1; Lots 1-30, Block 2; Lots 1-16, Block 3; Lots 1-13, Block 4; Lots 1-16, Block 5, being 31.364 acres situated in the W.M. Wilson Survey, County Abstract 1383, the D.A. Ware Survey, County Abstract 1580, and the W.M. Garrison Survey, County Abstract 0508 in the City of Corinth, Denton County, Texas.

APPROVAL PROCESS

The Planning and Zoning Commission shall consider and act on this item. If the preliminary plat and the associated preliminary construction plans meet the requirements of the City of Corinth, Denton County and State of Texas it should be approved.

STAFF RECOMMENDATION

Staff recommends that there be a motion to deny the preliminary plat contingent on the approval of the easement documents at City Council. Appropriate wording for this motion will be presented at the meeting if the Commission chooses to follow Staff recommendation.

AGENDA ITEM DESCRIPTION

Terrace Oaks' 62+ acre Master Planned Development Ordinance was approved by City Council in March 2015 and an amendment approval in June 2015. Sometime after the June amendment; Meritage Homes acquired the development project. Meritage took the proposed Parkland Dedication to City Council for approval in August 2015. Preliminary and final plat submittals for Terrace Oaks Phase I were received in October however, the applicant has been working to provide all the necessary information required by the UDC (Unified Development Code) to obtain Staff approval.

The Final Plat shows 108 residential buildable lots in what is proposed as Phase I. These lots range from 5,823 to 18,630 square feet. The Unified Development Code maintains "to insure adequate access to each Subdivision, there should be at least two (2) planned points of ingress and egress..." There is a planned secondary access at the end of Wellington Lane. The Development Review Committee (DRC) with the suggestion from the Deputy Fire Chief requested that there should be at least one temporary emergency access which the applicant has shown on Lot 3, Block 1 of the plat coming off of Post Oak Drive onto Alloway Drive; the separation does comply with the requirement as explained by the Deputy Chief.

As well; the City Attorney, City Staff and the Terrace Oaks Development Team are working on the off-site sanitary sewer easement and drainage easement documents that will need to go before City Council.

FINANCIAL SUMMARY

Source of Funding: No funding is required.

ATTACHMENTS / SUPPORTING DOCUMENTS

Terrace Oaks Final Plat

Submitted By: Barbara Cabbage, Planning and Development Manager, Department: Planning and Development

Finance Review: NA

Legal Review: In Process

Director Review and Approval:

BUSINESS AGENDA ITEM # 3

Planning and Zoning Commission Special Session

April 18, 2016

AGENDA ITEM

3. Consider and act on the Final Plat of Terrace Oaks Addition Phase I, Lots 1-33, Block 1; Lots 1-30, Block 2; Lots 1-16, Block 3; Lots 1-13, Block 4; Lots 1-16, Block 5, being 31.364 acres situated in the W.M. Wilson Survey, County Abstract 1383, the D.A. Ware Survey, County Abstract 1580, and the W.M. Garrison Survey, County Abstract 0508 in the City of Corinth, Denton County, Texas.

APPROVAL PROCESS

The Planning and Zoning Commission shall consider and act on this item. If the plat and the associated construction plans meet the requirements of the City of Corinth, Denton County and State of Texas it should be approved.

STAFF RECOMMENDATION

Staff recommends that there be a motion to deny the plat contingent on the approval of the easement documents at City Council. Appropriate wording for this motion will be presented at the meeting if the Commission chooses to follow Staff recommendation.

AGENDA ITEM DESCRIPTION

Terrace Oaks' 62+ acre Master Planned Development Ordinance was approved by City Council in March 2015 and an amendment approval in June 2015. Sometime after the June amendment; Meritage Homes acquired the development project. Meritage took the proposed Parkland Dedication to City Council for approval in August 2015. Preliminary and final plat submittals for Terrace Oaks Phase I were received in October however, the applicant has been working to provide all the necessary information required by the UDC (Unified Development Code) to obtain Staff approval.

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As well; the City Attorney, City Staff and the Terrace Oaks Development Team are working on the off-site sanitary sewer easement and drainage easement documents that will need to go before City Council.

FINANCIAL SUMMARY

Source of Funding: No funding is required.

ATTACHMENTS / SUPPORTING DOCUMENTS

Terrace Oaks Final Plat

Submitted By: Barbara Cabbage, Planning and Development Manager, Department: Planning and Development
Finance Review: NA Legal Review: In Process
Director Review and Approval:



Innovative approaches
Practical results
Outstanding service

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www.freese.com

April 7, 2016

Mr. Fred Gibbs
Planning and Development Director
City of Corinth
3300 Corinth Parkway
Corinth, TX 76208

Re: Terrace Oaks Final Plat and Construction Plans Review #4

Dear Mr. Gibbs:

Freese and Nichols, Inc. received the referenced submittal on March 30, 2016 and we have no comments at this time.

Sincerely,

A handwritten signature in blue ink that reads 'Rebecca Diviney'. The signature is stylized and includes a long, sweeping horizontal line that extends to the right.

Rebecca Diviney, P.E.
Engineer

PUBLIC HEARING / BUSINESS ITEM # 4 & 4a

Planning and Zoning Commission Special Session

April 18, 2016

AGENDA ITEM

4. TO HEAR PUBLIC OPINION REGARDING A REQUEST FROM THE APPLICANT DALE RIDINGER, WITH RIDINGER ASSOCIATES, INC, AUTHORIZED REPRESENTATIVE FOR THE PROPERTY OWNERS, JOHN HOFFLER AND SUSAN TORRIE FOR A ZONING CHANGE FROM SINGLE FAMILY-2 (SF-2) TO PLANNED DEVELOPMENT SINGLE FAMILY-3 PROVIDING A CONCEPT DESIGN MAP ON A TOTAL OF 2.658 ACRES LEGALLY DESCRIBED AS A PORTION OF J. HOFFLER ADDITION, BEING A PART OF A CERTAIN TRACT OF LAND DESCRIBED BY DEED TO JOHN V. HOFFLER RECORDED IN VOLUME 2853, PAGE 707 OF THE PRDCT AND BEING TRACT 3 SITUATED IN THE J.M. WALTON SURVEY, COUNTY ABSTRACT 1390 IN THE CITY OF CORINTH, DENTON COUNTY, TEXAS. (This property is located on the south side of Fritz Lane - 3618 Fritz Lane).
- 4a. Consider and act on a zoning change from Single Family-2 (SF-2) to Planned Development Single Family-3 on a total of 2.658 acres legally described as a portion of J. Hoeffler Addition, and being a part of a certain tract of land described by deed to John V. Hoeffler recorded in Volume 2853, Page 7-7 of the PRDCT and being Tract 3 situated in the J.M. Walton Survey, County Abstract 1390 in the City of Corinth, Denton County, Texas.

APPROVAL PROCESS

Public hearings will be held during the Planning and Zoning Commission meeting and the City Council meeting. A recommendation from the Planning and Zoning Commission regarding the zoning change request will be presented to City Council.

NOTIFICATION TO PUBLIC

Prior to the Planning and Zoning Commission meeting; fourteen property owners whose properties fall within 200' of the subject properties received letters noticing them of a public hearing. Public hearing signs were placed on Fritz Lane and Belle Way.

STAFF RECOMMENDATION

Staff recommends approval of the zoning change.

AGENDA ITEM SUMMARY/BACKGROUND

The zoning for the first phase of Parkside Farms was approved by City Council in April 2014. The Planning and Zoning Commission approved the preliminary plat for Parkside in May 2014 and the final plat in June 2014. Harwood Homes is the builder on this site and have ten homes under construction at this time.

Reginald Rembert, with Rembert Builders is the authorized representative for the property owners John Hoeffler and Susan Torrie. Mr. Rembert has requested a zoning change from SF-2 Single Family Residential to Planned Development SF-3 Single Family Residential. John Hoeffler and Susan Torrie have provided signatures on applications for zoning changes for 2.658 acres. The site is undeveloped aside from

an existing structure that is posed for demolition prior to the construction process. The developer Reginald Rembert has proposed a single family subdivision with Single Family-3 (SF-3) based zoning regulations mirroring that what was approved for the first phase of Parkside Farms.

SURROUNDING PROPERTIES ZONING

- Subject Property SF-2, Single Family Residential
- North SF-3, Single Family Residential
- South PD SF-2
- West SF-2, Single Family Residential
- East SF-2, Single Family Residential

SURROUNDING PROPERTIES EXISTING LAND USE

- Subject Property Low Density Residential / Undeveloped
- North Low Density Residential
- South Low Density Residential
- West Low Density Residential
- East Low Density Residential

SURROUNDING PROPERTIES FUTURE LAND USE

- Subject Property Low Density Residential
- North Low Density Residential
- South Low Density Residential
- West Low Density Residential
- East Low Density Residential

Parkside Farms Phase II:

- 2.658 Acres = 115,782.48 square feet
- 8 SF lots
- Minimum Lot size by proposed zoning: 10,000
- Minimum Lot size provided: 10,839
- Maximum Lot size: 13,678

| | SF-2 | SF-3 | Parkside Farms | Parks of Corinth Tract 1 / Tract 2 | Meadows North |
|-------------------|-------------------------|--|--|--|---------------|
| Front Yard | 25' | 25' | 25' | 25' / 25' | 25' |
| Side Yard | 15' Interior 25' Corner | 7.5' Interior 15' / 25' (side entry garage) Corner | * 7.5' Interior 15' / 25' (side entry garage) Corner | 10' / 5' Separation 30' / 25' 15' Corner | 5' |

| | | | | | |
|------------------------------|---------------------------------|-------------------------------|--------------------------------|--|--------------------------------|
| Rear Yard | 20' | 20' | 30' | 25' or 20' / 25' or 20' | 30% of depth or 30' max |
| Lot Area | 14,000 sf | 10,000 sf | 10,000 sf | 14,000 sf / 10,000 sf | 10,000 sf |
| Lot Width | 100' bldg. line / 80' prop line | 80' at building line | 60' prop line / 70' bldg. line | 100' / 80' at mid-point | 80' bldg. line / 60' prop line |
| Lot Depth | 110' | 100' | 100' | 110' / 110' | 100' |
| Minimum Dwelling Size | 2000 sf | 1700 sf | 2400 sf | 2000 sf / 2000 sf | 1500 sf |
| Lot Coverage | 30% - main and accessory comb | 30% - main and accessory comb | 40% - main and accessory comb | Max building area shall not exceed 4200 sf/3000 sf | 30% |

* Minimum side yard setbacks: 30 feet on the lots adjacent to the lots in the Parks of Corinth Addition, said lots depicted as Block B, Lot 5, Block C, Lots 1, 6, and 23 as shown on the preliminary plat for Parkside Farms Additon.

Parkside Farms Phase II Proposed Land Use Regulations:

PURPOSE: This district is intended to provide for development of single family dwelling units on lots of not less than ten thousand (10,000) square feet.

A. Permitted Uses and Use Regulations

The Permitted Uses in the SF-3, Single Family Residential, as listed in Subsection 2.07 shall be permitted uses and shall include the following:

B. Dimensional Regulations

The Dimensional Regulations described in Section 2.04.03 of the Unified Development Code, Ordinance No. 13-05-02-08, for the Single Family-3 (SF-3) District, as amended from time to time, shall be adhered to in its entirety for the purposes of development of the property except for the following:

1. Minimum Lot Width: 60' at the building line and 70' at the front property line.
2. Minimum Floor Area: 2400 square feet.
3. Maximum Building area: including any enclosed garages and porch area and accessory structures shall not exceed forty percent (40%) of total lot area.

C. Development Standards

The Development Standards for this PD shall comply with the development standards for SF-3 Single Family Residential, Section 2.04.03 of the City's Unified Development Code except as otherwise stated herein.

1. See UDC 2.07.07 Accessory Buildings and Uses.

2. See UDC 2.09.01 Landscape Regulations.
3. See UDC 2.09.02 Tree Preservation.
4. See UDC 2.09.03 Vehicle Parking Regulations.
5. See UDC 2.09.04 Building Façade Material Standards including the following:
 - a. The exterior facades of a main building or structure shall be constructed of one hundred (100) percent Class 1: Masonry Construction with no more than 85% of one product in this Single Family Residential District.
6. See UDC 2.09.07 Lighting and Glare Regulations.
7. See UDC 4.01 Sign Regulations.
8. See UDC 4.02 Fence and Screening Regulations.

CONFORMANCE TO THE COMPREHENSIVE PLAN

The proposed zoning conforms to the existing Future Land Use designations of the City of Corinth Comprehensive Plan – Low Density Residential.

FINANCIAL SUMMARY

No funding is required.

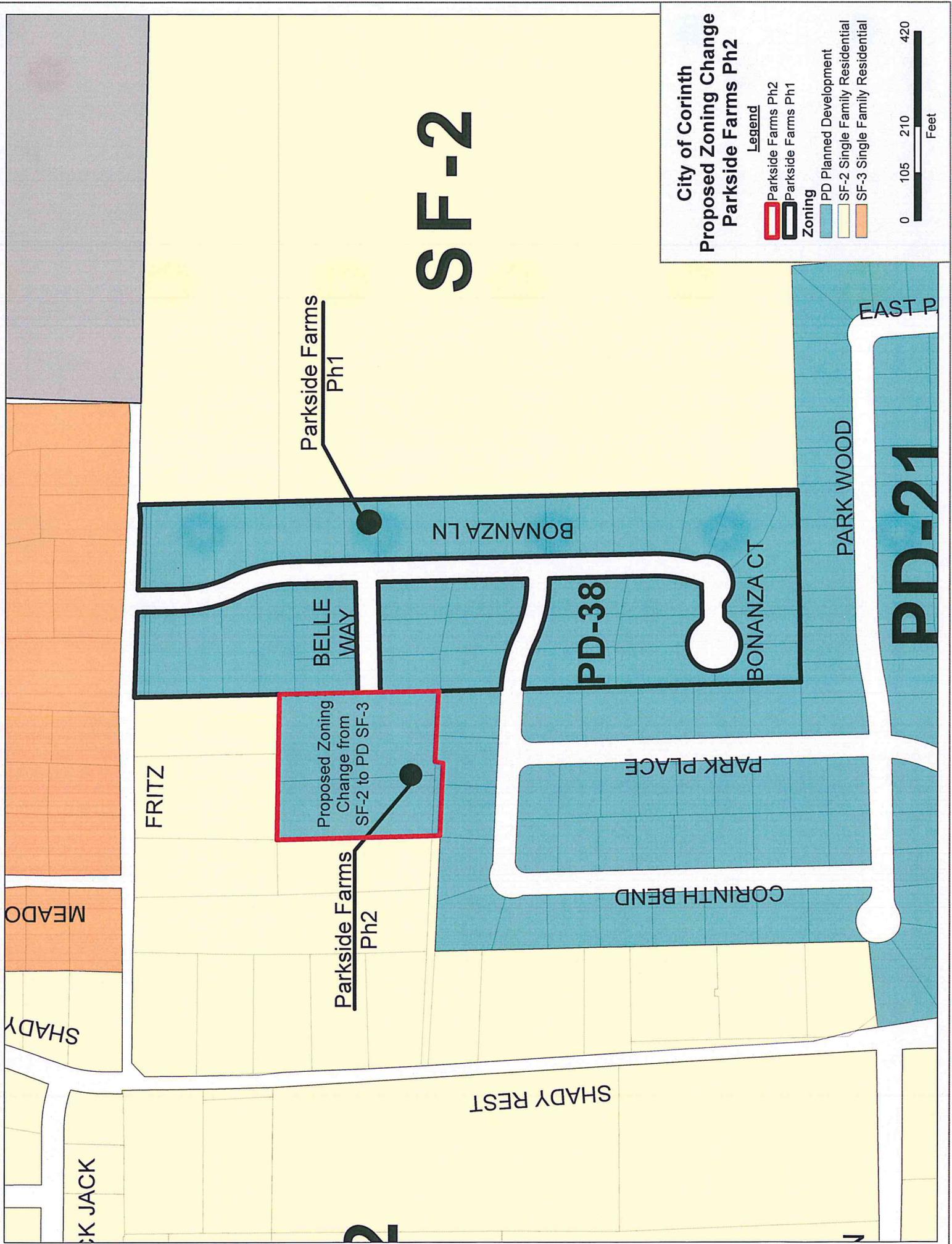
RECOMMENDATION

Staff recommends approval of the zoning change request.

ATTACHMENTS/SUPPORTING DOCUMENTS

Zoning Exhibit

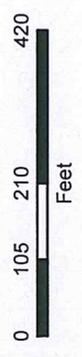
Submitted By: Barbara Cabbage
Department: Planning and Development
Finance Review: Yes ___ NA X
Legal Review: Yes X NA ___
Director Review and Approval:



SF-2

City of Corinth Proposed Zoning Change Parkside Farms Ph2

- Legend**
- Parkside Farms Ph2
 - Parkside Farms Ph1
- Zoning**
- PD Planned Development
 - SF-2 Single Family Residential
 - SF-3 Single Family Residential





2.06.03. PD, Planned Development

The purpose of this district is to encourage quality and better development in the City by allowing flexibility in the planning and development of projects. A [PD, Planned Development](#) District may be used to permit new or innovative concepts in land utilization or diversification that could not be achieved under conventional zoning approaches. Any combination of residential, commercial, light industrial, public or recreational uses as approved by the City Council may be permitted.

A. General Description and Purpose

1. The PD designation shall be used for the following purpose(s):
 - a. Master planning;
 - b. To carry out specific goals of the [Comprehensive Plan](#), City or public/private partnered special projects;
 - c. Development of mixed use, transit-oriented, or traditional neighborhoods with a variety of uses and housing types; and/or
 - d. To preserve natural features, open space, and other topographical features of the land.
2. The PD designation shall not be used for the following purpose(s):

To obtain [Zoning Variances](#) and [Subdivision Waivers](#) from existing development standards; to secure agreements between an [Applicant](#) and nearby property owners to receive zoning approval; and/or to assign responsibility to the City of private deed restrictions or covenants.

B. Land Area Requirement

1. A PD district requires a minimum of two (2) contiguous acres, except as allowed 2.06.03. B.2 below.
2. A PD district acreage may be less than two (2) acres when carrying out the recommendations of the [Comprehensive Plan](#).

C. Base Zoning District

1. A PD shall contain at least one base zoning district to regulate all uses and development regulations not identified as being modified.
2. If the standards of the base zoning district are amended, then the most recently amended standards shall apply to a PD district unless the standards have been individually listed within the PD adoption ordinance as being different from the base zoning district.
3. Base zoning district amendments are applicable to PD districts.

D. Permitted Uses and Use Regulations

1. See the [Use Chart](#) and all applicable regulations within Subsection 2.07.
2. See Section [2.10.09. PD, Planned Development Application and Review](#).

E. Dimensional Regulations

1. See [Subsection 2.08 Zoning Dimensional Regulations](#).
2. See Section [2.10.09. PD, Planned Development Application and Review](#).

F. Development Standards

1. See the [Zoning Development Regulations](#) in Subsection 2.09 for all applicable regulations.
2. See Section [2.10.09. PD, Planned Development Application and Review](#).

EXHIBIT "B"

LAND USE REGULATIONS

Parkside Farms Planned Development Single Family (PD SF) Residential District

PURPOSE: This district is intended to provide for development of single family dwelling units on lots of not less than ten thousand (10,000) square feet as shown on the approved site plan.

A. Permitted Uses and Use Regulations

The Permitted Uses in the SF-3, Single Family Residential, as listed in Subsection 2.07 shall be permitted uses.

B. Dimensional Regulations

The Dimensional Regulations described in Section 2.04.03 of the Unified Development Code, Ordinance No. 13-05-02-08, for the Single Family-3 (SF-3) District, as amended from time to time, shall be adhered to in its entirety for the purposes of development of the property except for the following:

1. Minimum Lot Width: 70' at the building line and 60' at the front property line.
2. Minimum Floor Area: 2400 square feet.
3. Maximum Building area: including any enclosed garages and porch area and accessory structures shall not exceed forty percent (40%) of total lot area.
4. Minimum side yard setbacks: 30 feet on the lots adjacent to the lots in the Parks of Corinth Addition, said lots depicted as Block B, Lot 6, Block C, Lots 1, 6, and 23 in the Preliminary Plat of the Parkside Farms Addition.

C. Development Standards

The Development Standards for this PD shall comply with the development standards for SF-3 Single Family Residential, Section 2.04.03 of the City's Unified Development Code except as otherwise stated herein.

1. See UDC 2.07.07 **Accessory Buildings and Uses.**
2. See UDC 2.09.01 **Landscape Regulations.**
3. See UDC 2.09.02 **Tree Preservation.**
4. See UDC 2.09.03 **Vehicle Parking Regulations.**
5. See UDC 2.09.04 **Building Façade Material Standards** including the following:
6. The exterior facades of a main building or structure shall be constructed of one hundred (100) percent Class 1: Masonry Construction with no more than 85% of one product.
7. See UDC 2.09.07 **Lighting and Glare Regulations.**
8. See UDC 4.01 **Sign Regulations.**



December 1, 2015

Mr. Fred Gibbs
Director of Planning and Development
City of Corinth
3300 Corinth Pkwy.
Corinth, Texas 76208

Re: Parkside Farms Phase 2 Subdivision-Zoning

Dear Mr. Gibbs:

Ridinger Associates, Inc. is pleased to inform you and the City that our client, Reginald Rembert, intends to develop an 8 lot, PD residential subdivision on a 2.589 acre tract in the City of Corinth.

This tract is located on the west side of Parkside Farms and adjacent to the north side of the Parks of Corinth subdivision. The development will be in accordance with the PD-38 zoning. It is the intention of our client to construct this subdivision as a single phase.

We appreciate your consideration to our project and look forward to your favorable approval. Please call if you have any questions or comments.

Thank You,

Dale Ridinger,
President

Cc: file

EXHIBIT 'B'
AMENDED ZONING MAP

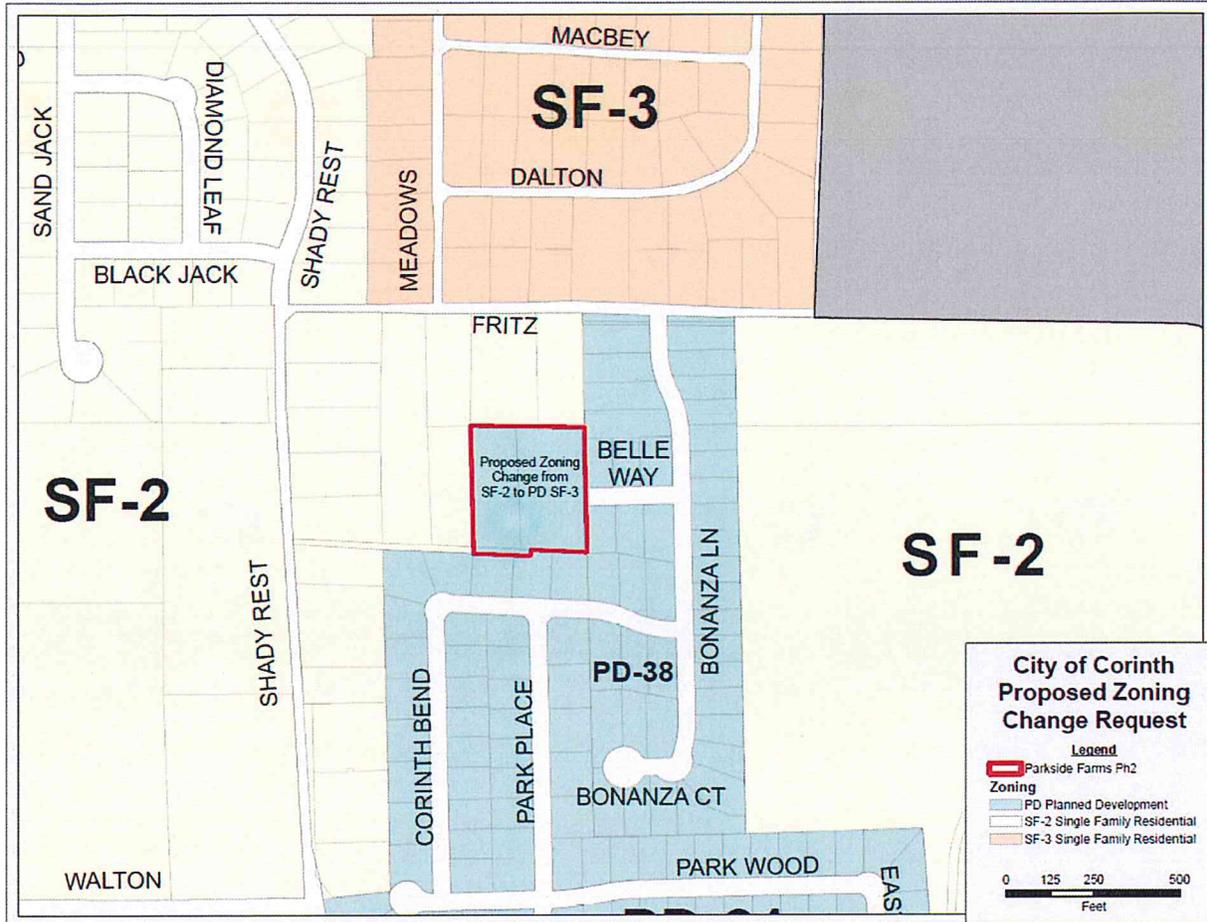


EXHIBIT C
PD CONCEPT DESIGN STATEMENT

Statement of Purpose: The purpose of this PD ordinance is to allow for the development and expansion of Parkside Farms; a Planned Development Single Family-3 residential subdivision. The purpose of these general requirements and stipulations is to provide a framework for quality development consistent with City of Corinth zoning criteria and to provide consideration for existing properties adjacent to this property.

This planned development shall be called, Parkside Farms Phase II. The developer, Reginald Rembert is developing the property that is located west of Parkside Farms at the end of Belle Way and is generally located south of Fritz Lane and north of Corinth Parkway. Its border the Parks of Corinth to the south a single family residential subdivision zoned Planned Development SF-3. The project site is currently zoned SF-2 with zoning to the north SF-2. Zoning on the north side of Fritz Lane is SF-3. Part of the project site is undeveloped and part is a portion of a larger single family tract.

Proposed zoning is Planned Development Single Family-3. The proposed site layout includes eight lots ranging in size of 10,839 square feet to 13, 678 square feet.

SF-3 Residential zoning district has been selected as the base district for the overall development. (See "Exhibit A" and "Exhibit 'B' for descriptions and a map of the proposed districts)

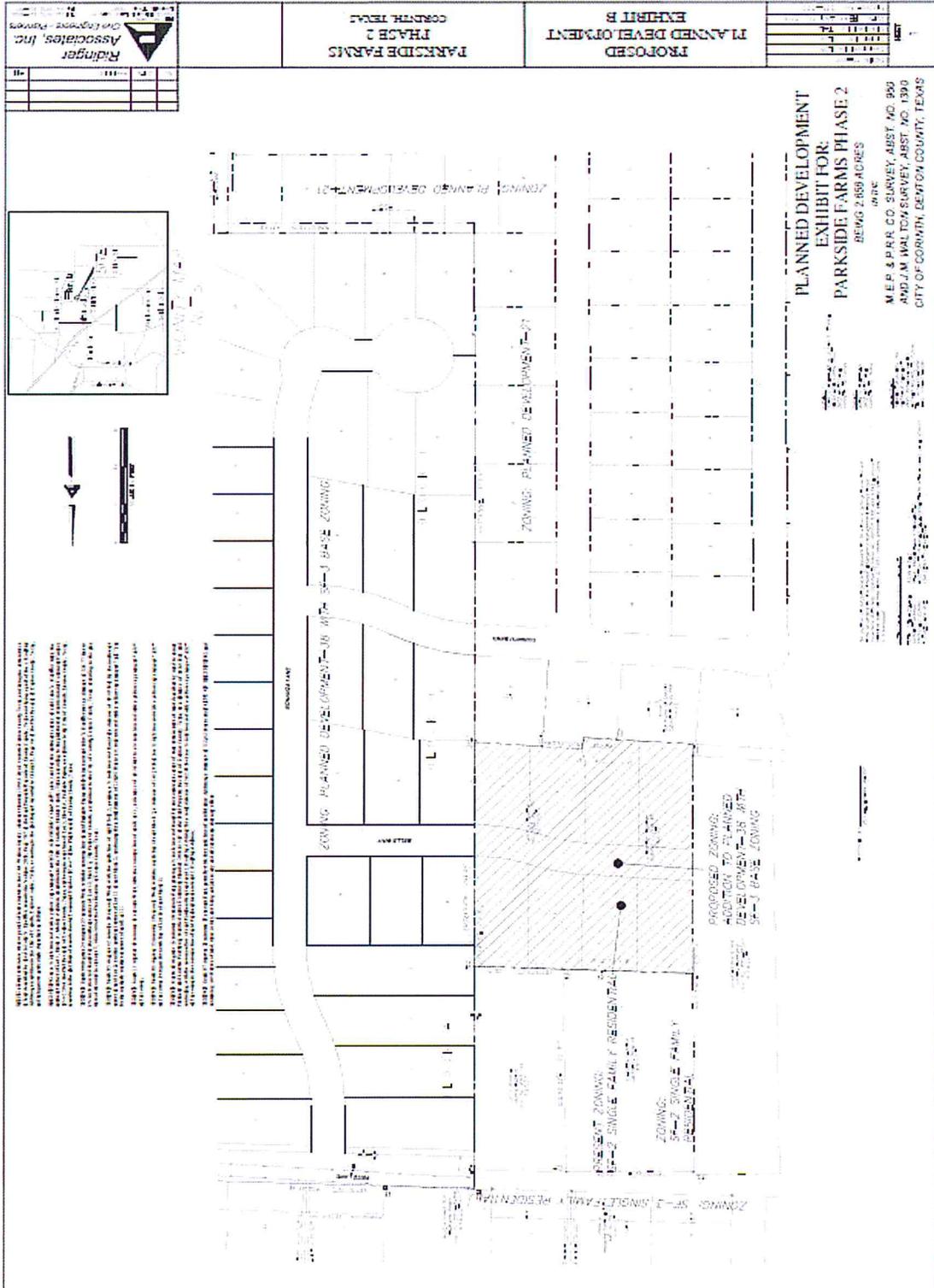
A Major Waiver to the Subdivision Regulations in City of Corinth Ordinance No. 13-05-08-20, Unified Development Code (UDC) Section 3.05.13 F to allow paving 80 foot diameter with 81 foot total easement temporarily in the proposed Phase II of Parkside Farms Lots at the end of the Belle Way.

Existing and Proposed Streets around the development in question are:

- North (Fritz Lane)
- West (Bonanza)
- East (Shady Rest Lane)
- South (Corinth Parkway)
- An extension of Belle Way is proposed.

The existing site drains south. The proposed development will be directed into the right-of-way of Belle Way. Proposed public utility lines will continue from Phase I and will extend to the end of the proposed development.

**EXHIBIT D
 CONCEPT DESIGN MAP**



**PLANNED DEVELOPMENT
 EXHIBIT FOR:
 PARKSIDE FARMS PHASE 2
 BEING 2.680 ACRES**

NOTE
 M.E.P. & P.R.R. CO. SURVEY, ABST. NO. 895
 AND J.M. WALTON SURVEY, ABST. NO. 1390
 CITY OF CORINTH, DEWTON COUNTY, TEXAS

**EXHIBIT “E”
LAND USE REGULATIONS**

Parkside Farms Phase II Planned Development Single Family (PD SF-3) Residential District

PURPOSE: This district is intended to provide for development of single family dwelling units on lots of not less than ten thousand (10,000) square feet as shown on the approved site plan.

A. Permitted Uses and Use Regulations

The Permitted Uses in the SF-3, Single Family Residential, as listed in Subsection 2.07 shall be permitted uses.

B. Dimensional Regulations

The Dimensional Regulations described in Section 2.04.03 of the Unified Development Code, Ordinance No. 13-05-02-08, for the Single Family-3 (SF-3) District, as amended from time to time, shall be adhered to in its entirety for the purposes of development of the property except for the following:

1. Minimum Lot Width: 60’ at the building line and 70’ at the front property line.
2. Minimum Floor Area: 2400 square feet.
3. Maximum Building area: including any enclosed garages and porch area and accessory structures shall not exceed forty percent (40%) of total lot area.

C. Development Standards

The Development Standards for this PD shall comply with the development standards for SF-3 Single Family Residential, Section 2.04.03 of the City’s Unified Development Code except as otherwise stated herein.

1. See UDC 2.07.07 **Accessory Buildings and Uses.**
2. See UDC 2.09.01 **Landscape Regulations.**
3. See UDC 2.09.02 **Tree Preservation.**
4. See UDC 2.09.03 **Vehicle Parking Regulations.**
5. See UDC 2.09.04 **Building Façade Material Standards** including the following:
6. The exterior facades of a main building or structure shall be constructed of one hundred (100) percent Class 1: Masonry Construction with no more than 85% of one product as allowed in the Single Family Residential District.
7. See UDC 2.09.07 **Lighting and Glare Regulations.**
8. See UDC 4.01 **Sign Regulations.**
9. See UDC 4.02 **Fence and Screening Regulations** shall apply.



Planning and Zoning Commission Meeting Date: MONDAY, MARCH 28, 2016 AT 7:00 P.M.

Dear Property Owner:

The Planning and Zoning Commission of the City of Corinth, Texas, will hold a Public Hearing on **Monday, March 28, 2016 at 7:00 p.m.** in the Corinth City Hall located at 3300 Corinth Parkway, Corinth, Texas, to hear public opinion regarding the following:

A request from the applicant Dale Ridinger with Ridinger Associates, Inc. authorized representative for the property owners John Hoeffler and Susan Torrie for a zoning change from Single Family-2 (SF-2) to Planned Development Single Family providing a Concept Design Map on a total of 2.658 acres legally described as a portion of J. Hoeffler Addition (1.128 acres) and 1.53 acres being Tract 3 situated in the J.M. Walton Survey, County Abstract 1390 in the City of Corinth, Denton County, Texas. This property is located on the south side of Fritz Lane (3618 Fritz Lane).

Per the City of Corinth Unified Development Code (UDC) found on the City of Corinth website and available for review at City Hall; as a property owner within two hundred (200) feet of this property, you are invited to attend this meeting. You are not required to be present, but all interested parties wishing to be heard should appear at the time and place stated above.

Your opinion regarding the request to rezone the property described above may be expressed by notation on this form or by letter. You may support or oppose the requested zoning change; your opposition will be considered a protest.

I oppose the requested zoning change.

Name/Address Stefanie Powell / 3607 Fritz Lane

Please don't hesitate to call if you have any questions regarding the zoning change request. I may be reached at (940) 498-3295.

Sincerely,

Barbara Cabbage, Planning and Development Manager
City of Corinth, Texas

BUSINESS ITEM # 5

Planning and Zoning Commission Special Session

April 18, 2016

AGENDA ITEM

5. Consider and act on a request from the applicant, Dale Ridinger with Ridinger Associates, Inc, authorized representative for the property owner, for a Major Subdivision Waiver to City of Corinth Ordinance No. 13-05-08-20, Unified Development Code (UDC) Section 3.05.13 F to allow paving 80 foot diameter with 81 foot total easement temporarily in the proposed Phase II of Parkside Farms Lots at the end of the proposed "Belle Way" located on property legally described as 2.658 acres being a portion of J. Hoeffler Addition, and being a part of a certain tract of land described by deed to John V. Hoeffler recorded in Volume 2853, Page 7-7 of the PRDCT and being Tract 3 situated in the J.M. Walton Survey, County Abstract 1390 in the City of Corinth, Denton County, Texas.

APPROVAL PROCESS

The Planning and Zoning Commission shall consider this item and make a recommendation to City Council.

STAFF RECOMMENDATION

The Development Review Committee (DRC) has reviewed the request and the associated documents. The Fire Marshall has determined that the dimensions proposed will allow for proper maneuvering of the emergency apparatus and that the Lake Cities Fire Department could provide necessary services to these lots safely and adequately during emergency situations as the street is currently proposed. Staff recommends approval Major Waiver as requested.

AGENDA ITEM DESCRIPTION

The Planning and Zoning Commission recommended approval of the original Planned Development Zoning Change submittal of Parkside Farms addition in March 2014. The Preliminary and Final Plats were approved in May and June of 2014. The infrastructure has been constructed and new single family residential homes have been permitted and are being built.

With the extension of Belle Way the street would dead end at a distance from the nearest intersection beyond that allowed for a "dead end" restrictions in the City of Corinth Unified Development Code Subdivision Regulations.

FINANCIAL SUMMARY

Source of Funding: No funding is required.

ATTACHMENTS / SUPPORTING DOCUMENTS

Easement documents

Submitted By: Barbara Cubbage, Planning and Development Manager, Department: Planning and Development

Finance Review: NA

Legal Review: NA

Director Review and Approval:



March 7, 2016

Mr. Fred Gibbs
Director of Planning and Development
City of Corinth
3300 Corinth Pkwy.
Corinth, Texas 76208

Re: Parkside Farms Phase 2 Subdivision Waiver

Dear Mr. Gibbs:

In a meeting with City staff it was determined that in order to extend Belle Way westward with a cul-de-sac with a turnaround with an outside paving diameter of 80 feet, and a temporary turn around easement with a diameter of 81', we would have to request a major subdivision waiver. Please consider this letter as a request for a waiver to Section 3.05.13 F Cul-de-sacs and dead end streets to allow:

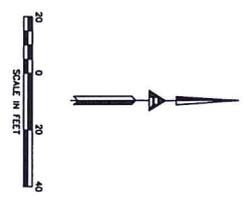
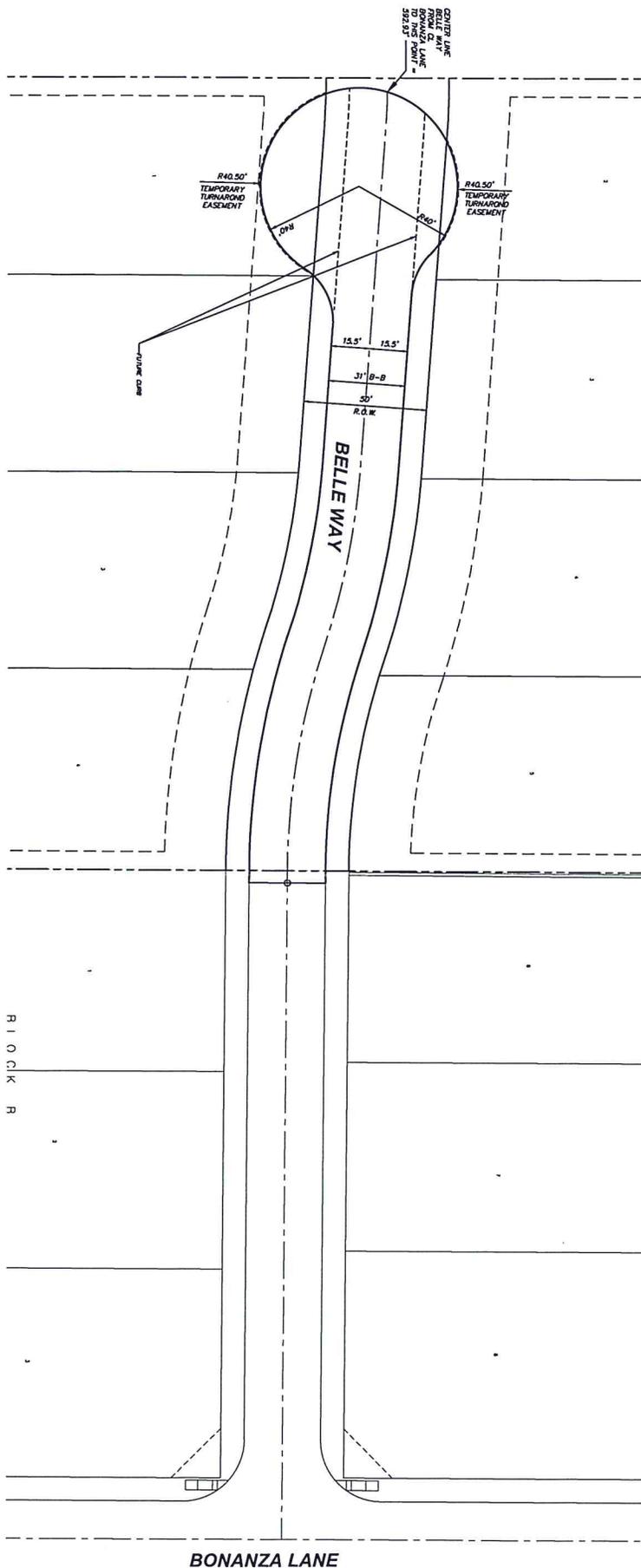
- 1.) An 80 foot turnaround outside paving diameter in lieu of the 100 foot outside paving diameter as required in the Unified Development Code for the concrete paved cul-de-sac at the closed end.
- 2.) An 81 foot diameter temporary turnaround easement in lieu of the 120 feet dimension as listed for the Right-of-Way in the Unified Development code.

We appreciate your consideration to our project and look forward to your favorable approval. Please call if you have any questions or comments.

Thank You,

Dale Ridinger,
President

Cc: file



BONANZA LANE

Scale: 1"=40' HOR., 1"=4' VERT.
 Designed by: JRK
 Drawn by: JRK
 Checked by: TAL
 Date: FEBRUARY 15, 2016
 Project No. 035-020

SUBDIVISION WAIVER
 EXHIBIT

PARKSIDE FARMS
 PHASE 2
 CORINTH, TEXAS

| No. | Date | Revisions | App. |
|-----|------|-----------|------|
| | | | |

Ridinger Associates, Inc.
 Civil Engineers - Planners
Firm No. 1999
 300 S. Enterprise Lane, Suite 101
 LeVittville, Texas 75087
Tel. No. (972) 353-8200
 Fax No. (972) 353-8011

BUSINESS AGENDA ITEM # 6

Planning and Zoning Commission Special Session

April 18, 2016

BUSINESS AGENDA ITEM

6. Consider and act on the Preliminary Plat of Parkside Farms Phase II Lots 1-4, Block E and Lots 1-4, Block F being 2.658 acres situated in the J.M. Walton Survey, County Abstract 1390 including a portion of J. Hoeffler Addition recorded in Cabinet L, Page 46 PRDCT in the City of Corinth, Denton County, Texas.

APPROVAL PROCESS

The Planning and Zoning Commission shall consider and act on this item. If the plat and the associated construction plans meet requirements of the City of Corinth, Denton County and State of Texas it should be approved.

STAFF RECOMMENDATION

Staff recommends that there be a motion to deny the preliminary plat contingent on receiving the blanket easement letter from the utility company and approval of the rezoning request and Major Subdivision Waiver at City Council. Appropriate wording for this motion will be presented at the meeting should the Commission choose to follow Staff recommendation.

AGENDA ITEM DESCRIPTION

The zoning for the first phase of Parkside Farms was approved by City Council in April 2014. The Planning and Zoning Commission approved the preliminary and final plats for Parkside in March 2014. Harwood Homes is the builder on this site and have ten homes under construction at this time.

Reginald Rembert, with Rembert Builders is the authorized representative for the property owners John Hoeffler and Susan Torrie. Mr. Rembert has requested a zoning change from SF-2 Single Family Residential to Planned Development SF-3 Single Family Residential. If the Planning and Zoning Commission recommends approval of the zoning change request and the Major Subdivision Waiver then it would be appropriate for the Commission to act on the plats. However the plats would be denied contingent on City Council approval of an ordinance associated with the acceptance of the zoning change and the Major Subdivision Waiver. As well, the applicant has been working with the City Attorney, City Staff and the utility company responsible for the blanket easement on the subject property. So there would be a contingency on the acceptance of that easement by the utility company and receipt of the letter to the City allowing the delineated easement.

FINANCIAL SUMMARY

Source of Funding: No funding is required.

ATTACHMENTS / SUPPORTING DOCUMENTS

Preliminary Plat

Submitted By: Barbara Cubbage, Planning and Development Manager, Department: Planning and Development

Finance Review: NA

Legal Review: Active

Director Review and Approval:

PUBLIC HEARING / BUSINESS AGENDA ITEM # 7 & 7a

Planning and Zoning Commission Special Session

April 18, 2016

BUSINESS AGENDA ITEM

7. TO HEAR PUBLIC OPINION REGARDING A FINAL PLAT OF PARKSIDE FARMS PHASE II LOTS 1-4, BLOCK E AND LOTS 1-4, BLOCK F BEING 2.658 ACRES SITUATED IN THE J.M. WALTON SURVEY, COUNTY ABSTRACT 1390 INCLUDING A REPLAT OF A PORTION OF J. HOEFFLER ADDITION RECORDED IN CABINET L, PAGE 46 PRDCT IN THE CITY OF CORINTH, DENTON COUNTY, TEXAS. (his property is located at 3618 Fritz Lane (south side of Fritz and at the end of Belle Way.)
- 7a. Consider and act on the Final Plat of Parkside Farms Phase II Lots 1-4, Block E and Lots 1-4, Block F being 2.658 acres situated in the J.M. Walton Survey, County Abstract 1390 including a Replat of a portion of J. Hoeffler Addition recorded in Cabinet L, Page 46 PRDCT in the City of Corinth, Denton County, Texas.

APPROVAL PROCESS

The Planning and Zoning Commission shall consider and act on this item. If the plat and the associated construction plans meet requirements of the City of Corinth, Denton County and State of Texas it should be approved.

STAFF RECOMMENDATION

Staff recommends that there be a motion to deny the Final Plat contingent on receiving the blanket easement letter from the utility company and approval of the rezoning request at City Council as well as approval of the Major Subdivision Waiver. Appropriate wording for this motion will be presented at the meeting should the Commission choose to follow Staff recommendation.

AGENDA ITEM DESCRIPTION

The zoning for the first phase of Parkside Farms was approved by City Council in April 2014. The Planning and Zoning Commission approved the preliminary and final plats for Parkside in March 2014. Harwood Homes is the builder on this site and have ten homes under construction at this time.

Reginald Rembert, with Rembert Builders is the authorized representative for the property owners John Hoeffler and Susan Torrie. Mr. Rembert has requested a zoning change from SF-2 Single Family Residential to Planned Development SF-3 Single Family Residential. If the Planning and Zoning Commission recommends approval of the zoning change request and the Major Subdivision Waiver then it would be appropriate for the Commission to act on the plats. However the plats would be denied contingent on City Council approval of an ordinance associated with the acceptance of the zoning change and the Major Subdivision Waiver. As well, the applicant has been working with the City Attorney, City Staff and the utility company responsible for the blanket easement on the subject property. So there would be a contingency on the acceptance of that easement by the utility company and receipt of the letter to the City allowing the delineated easement.

FINANCIAL SUMMARY

Source of Funding: No funding is required.

ATTACHMENTS / SUPPORTING DOCUMENTS

Final Plat

Submitted By: Barbara Cabbage, Planning and Development Manager, Department: Planning and Development
Finance Review: NA Legal Review: Active Director Review and Approval:



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www.freese.com

April 7, 2016

Mr. Fred Gibbs
Planning and Development Director
City of Corinth
3300 Corinth Parkway
Corinth, TX 76208

Re: Parkside Farms Phase 2 – Review #4 of Final Plat and Construction Plans

Dear Mr. Gibbs:

Freese and Nichols, Inc. received the referenced submittal on April 6, 2016, and we have no further comments.

Sincerely,

A handwritten signature in blue ink, appearing to read 'Rebecca P. Diviney'.

Rebecca P. Diviney, P.E.
Engineer

BUSINESS AGENDA

ITEM #8

Planning and Zoning Commission Special Session

April 18, 2016

AGENDA ITEM

BUSINESS: Consider and act on a request from the applicant Steve Homeyer, authorized representative for the property owner, Redriver Bancorp for a major subdivision waiver to the City of Corinth Ordinance No. 13-05-08-20, Unified Development Code (UDC) to allow a reduction in the minimum required distance from the intersection along a collector to the centerline of a proposed driveway on property legally described as part of Tract 10A being 2.21 acres out of the J.P. Walton Survey, Abstract No. 1389, in the City of Corinth, Denton County, Texas.

AGENDA ITEM DESCRIPTION

The applicant is requesting a major subdivision waiver in order to reduce the minimum required distance from the intersection of N. Corinth Street and Walton Drive to the centerline of the driveway into the proposed Domino's and future retail site. Section **3.05.04 Access Management** of the Unified Development Code requires the minimum distance from the intersection along City maintained collector roadways to the centerline of the driveway to be a minimum of 150' feet; whereas the applicant is proposing a minimum distance of 146' feet.

The proposed development is located at both the corner of N. Corinth Street and Walton Drive and I-35E and N. Corinth Street. Therefore, only one driveway is proposed along N. Corinth Street, as the site does not have sufficient room between these two intersections to meet the driveway spacing requirements to allow for additional driveways.

FINANCIAL SUMMARY

Source of Funding: No funding is required.

STAFF RECOMMENDATION

Staff recommends **Approval** of the request as presented for the following reasons:

1). the distance of the proposed driveway spacing from the City maintained intersection is only being reduced by 4' feet. 2). If the proposed driveway were located 150' from this intersection, it would not meet the required distance from the major intersection of the State-maintained I-35E arterial. 3). TXDOT is also supportive of this request. 4). Neither staff nor TXDOT could support a reduction in the required distance from the other intersection at I-35E and N. Corinth Street for this property and access must be provided.

ATTACHMENTS/SUPPORTING DOCUMENTS

Waiver Application
Exhibit

Submitted By: Lori Levy, AICP
Department: Planning and Development

Finance Review: Yes ___ NA X

Legal Review: Yes ___ NA X

Director Review and Approval:

APPLICATION FOR MAJOR SUBDIVISION WAIVER

SUBMITTAL DATE: 4-7-16

TO: PLANNING AND ZONING COMMISSION AND THE CITY COUNCIL
CITY OF CORINTH, TEXAS

APPLICANT'S NAME: Steven R. Homeyer, P.E.

APPLICANT'S ADDRESS: PO Box 294527, Lewisville, TX 75029

APPLICANT'S PHONE #: 972-906-9985 EMAIL: SHomeyer@hei.us.com

PROPERTY OWNER'S NAME Red River Bancorp, Inc.

OWNER'S ADDRESS: PO Box 10, Gainesville, TX 76241

LOCATION OF PROPERTY: East side of N. Corinth Street between Walton and
the north bound access road of I-35

LEGAL DESCRIPTION OF PROPERTY: LOT / BLOCK: Lot 1/Block A

ADDITION / SUBDIVISION G4 Corinth Addition

TRACTS: ABSTRACT / SURVEY: JP Walton Survey, AB 1389

OF ACRES: 0.97

For properties not in a recorded subdivision, submit a copy of a current survey or plat showing the properties proposed to be changed, and a complete legal field note description

(Tenant Name) (Tenant's Telephone Number)

(Tenant's Address – Street, City & Zip)

In accordance with the provisions of the Subdivision Ordinance, appeal is now made to the Planning and Zoning Commission / City Council to grant the following waiver request:

We are requesting a waiver to the Subdivision Ordinance requiring a distance of 150 LF between the driveway to the property and the right-of-way for Walton Drive. Our driveway is currently 146 LF from the right-of-way.

In order to grant a major waiver, the Planning and Zoning Commission / City Council must determine that ALL of the following conditions exist. **Financial hardship to the applicant alone is not sufficient reason to approve a waiver.* State how your request meets these conditions.

- a) That there are special circumstances or conditions affecting the land that when provisions of the ordinance are applied would deprive the applicant of reasonable use of the land.

If the entrance were moved further south, it would require a variance for the intersection with the I-35 access road. The driveway was placed to avoid conflict with that more critical intersection.

- b) That the waiver is necessary for the preservation and enjoyment of a substantial property right.

This driveway will provide the only access available to this property and is necessary to move forward with the project.

- c) That granting the waiver will not be detrimental to the public health, safety, or welfare, or injurious to the other property in the area.

As the driveway is only 4 feet short of the required separation distance, it poses no detrimental effects in any manner.

- d) That the waiver when granted is in harmony with the general purpose and intent of the ordinance or its amendments.

This waiver will allow the property to be developed without conflicting with the intersection of N. Corinth Street and the I-35 access road.

I do hereby certify that the above statements are true and correct to the best of my knowledge.



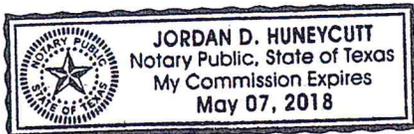
 (Applicant's Signature)

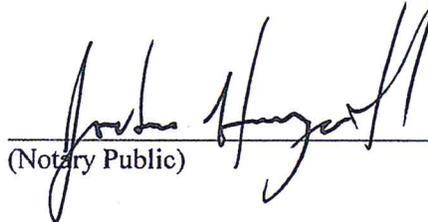
STATE OF TEXAS
 COUNTY OF DENTON

Subscribed and sworn to before me this 07 day of APRIL,

20 16, by JORDAN D. HUNEYCUTT, who on his /her oath certifies that

the above statements are true and correct to the best of his/her knowledge,





 (Notary Public)

Authorization of Agent

We, the undersigned, being owners of subject real property, do hereby authorize to act as our Agent in the matter of this request. The term agent shall be construed to mean any lessee, developer, option holder, or authorized individual who is authorized to act in behalf of the owner(s) of said property. (Please print.):

NAME: Marty Rivers / Red River Bancorp

ADDRESS: PO Box 10 Gainesville, TX 76241

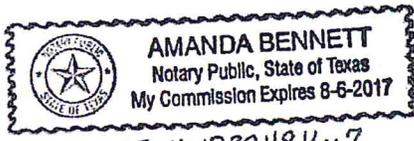
(Form to be signed below by all owners of the property and notarized.)

Printed Name / Signature

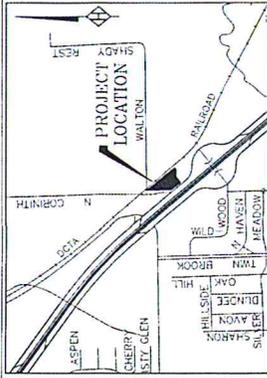
Mailing Address

Marty Rivers / Marty Rivers PO Box 10 Gainesville, TX 76241

Subscribed and sworn to before me on this 7th day of April, 2016.



Amanda R Bennett
(Notary Public)



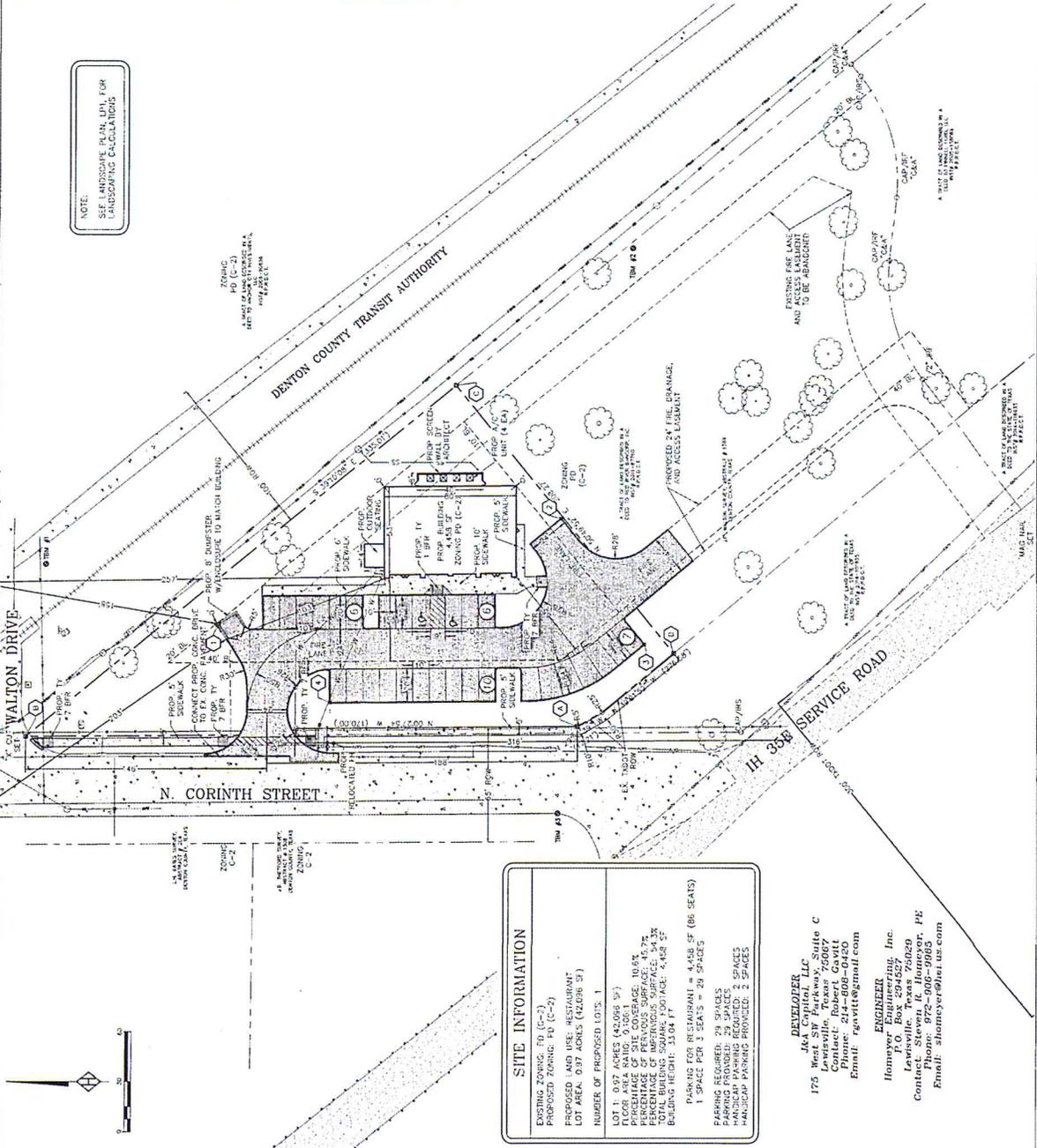
VICINITY MAP
 SCALE: 1" = 1000'

| PI | NORTHING | EASTING | PI | NORTHING | EASTING |
|----|-------------|--------------|----|-------------|--------------|
| 1 | 702654.5811 | 246720.3328 | 2 | 702658.8919 | 246781.9187 |
| 2 | 702657.9218 | 2466670.4749 | 3 | 712554.4332 | 2469321.4851 |
| 3 | 702676.1916 | 246832.0752 | 4 | 702594.3556 | 246775.8521 |
| 4 | 702654.6446 | 246870.5884 | 5 | 716274.3556 | 246874.0374 |

LEGEND

- RF: IRON ROD FOUND
- RS: IRON ROD SET
- 3": 3" OUT X IN CONCRETE
- FCP: FENCE CORNER POST
- ASPH: ASPHALT
- TEL: TELEPHONE MARHOLE
- FP: POWER POLE
- LF: LIGHT POLE
- WV: WATER VALVE
- SSW: SANITARY SEWER MANHOLE
- FF: FERE HYDRANT
- WM: WATER METER

PREPARED BY: ELMS
 THIS DOCUMENT IS FOR INTERIM REVIEW AND IS NOT INTENDED FOR CONSTRUCTION. NO BIDDING OR PERMIT PURPOSES.
 STEVEN R. HOMMEYER, PE # 86942
 DATE: 04/08/2016



NOTE: SEE LANDSCAPE PLAN FOR LANDSCAPING CALCULATIONS

SITE INFORMATION

EXISTING ZONING: FD (C-2)
 PROPOSED ZONING: FD (C-2)
 PROPOSED AND USE: RESTAURANT
 LOT AREA: 0.97 ACRES (42,036 SF)
 NUMBER OF PROPOSED LOTS: 1
 LOT 1: 0.97 ACRES (42,036 SF)
 FLOOR AREA RATIO: 0.100:1 USE: 10,477 SF
 PERCENTAGE OF IMPERVIOUS SURFACE: 16.7%
 PERCENTAGE OF PERVIOUS SURFACE: 83.3%
 BUILDING HEIGHT: 12.00 FT
 BUILDING HEIGHT: 12.00 FT
 PAVEMENT FOR RESTAURANT = 4,458 SF (66 SEATS)
 1 SPACE PER 3 SEATS = 28 SPACES
 PARKING REQUIRED: 28 SPACES
 PARKING PROVIDED: 29 SPACES
 HANDICAPPED PARKING REQUIRED: 2 SPACES
 HANDICAPPED PARKING PROVIDED: 2 SPACES

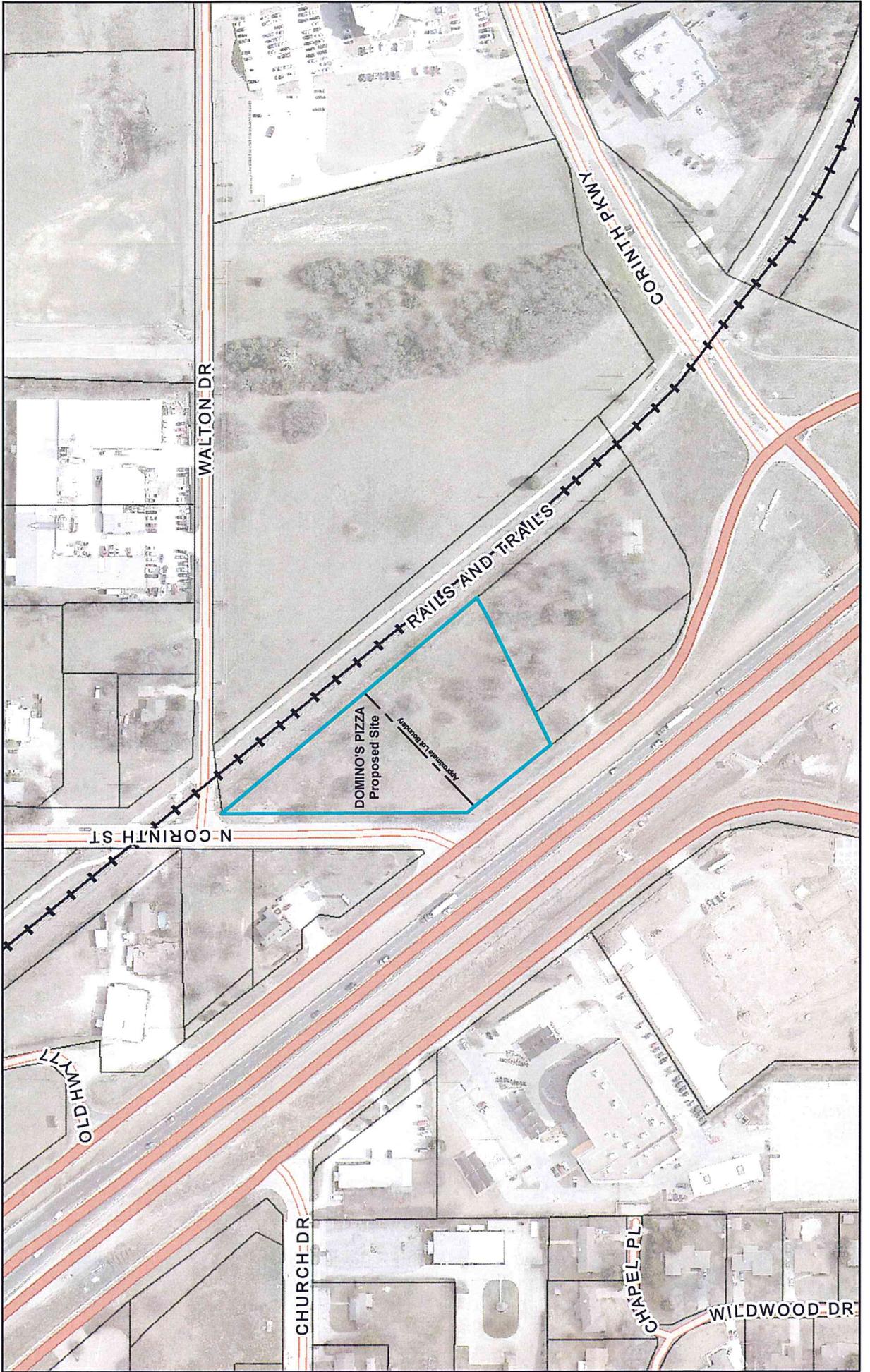
DEVELOPER
 JK Capital, LLC
 175 West Street, Suite C
 Lewisville, Texas 75067
 Contact: Robert Gavitt
 Email: rgavitt@gmail.com

ENGINEER
 Hommeyer Engineering, Inc.
 P.O. Box 294527
 Lewisville, Texas 75029
 Contact: Steven R. Hommeyer, PE
 Email: shommeyer@hei.us.com



CITY OF CORINTH

DOMINO'S PIZZA LOCATION MAP



PUBLIC HEARING / BUSINESS ITEM # 9 & 9a

Planning and Zoning Commission Special Session

April 18, 2016

AGENDA ITEM

9. TO HEAR PUBLIC OPINION REGARDING A REQUEST FROM THE APPLICANT, KYLE VRLA, AUTHORIZED REPRESENTATIVE FOR THE PROPERTY OWNER, MONICA UTTER, WITH UTTER PROPERTIES, FOR A ZONING CHANGE ON 12.283 ACRES FROM C-3, COMMERCIAL TO PLANNED DEVELOPMENT C-3 ON PROPERTY LEGALLY DESCRIBED AS BILL UTTER FORD ADDITION BLOCK A, LOT 1 AND A ZONING CHANGE ON APPROXIMATELY 6.255 ACRES FROM C-2, COMMERCIAL TO PLANNED DEVELOPMENT C-2 ON PROPERTY LEGALLY DESCRIBED AS BILL UTTER FORD ADDITION BLOCK A, LOT 2 AND TRACT 26 OUT OF THE L. BATES SURVEY, COUNTY ABSTRACT 204 IN THE CITY OF CORINTH, DENTON COUNTY, TEXAS.
- 9a. Consider and act on a zoning change on 12.283 acres from C-3, Commercial to Planned Development C-3 on property legally described as Bill Utter Ford Addition Block A, Lot 1 AND a zoning change on approximately 6.255 acres from C-2, Commercial to Planned Development C-2 on property legally described as Bill Utter Ford Addition Block A, Lot 2 and Tract 26 out of the L. Bates Survey, County Abstract 204 in the City of Corinth, Denton County, Texas.

APPROVAL PROCESS

Public hearings will be held during the Planning and Zoning Commission meeting and the City Council meeting. A recommendation from the Planning and Zoning Commission regarding the zoning change request will be presented to City Council.

The Planning and Zoning Commission recommendation will go before City Council for final action at the May 5, 2016 City Council regular session meeting.

NOTIFICATION TO PUBLIC

Prior to the Planning and Zoning Commission meeting; there are five property owners whose properties fall within 200' of the subject properties. Public hearing signs were placed on Lake View and IH 35E.

STAFF RECOMMENDATION

“Automobile Dealership New and Used” is an allowed use in the C-3, Commercial zoning district. It is allowed by Specific Use Permit in the C-2, Commercial Zoning District. The proposed uses are appropriate at this location, and conform to the Comprehensive Plan Future Land Use designation. Staff recommends approval of the proposed rezoning request with the following recommendation:

- Although a lighting plan is not required with this zoning change request; the lighting plan reviewed by staff showed that the foot candles existing currently on site are significantly higher than the level foot candles allowed by the UDC. The applicant is asking for leverage on leaving the existing lighting as is unless the main building is increased by 25%. The ordinance sets that limit at 20% and the applicant is at 24%. It is requested that the applicant research the lighting to find a solution to lowering the foot candles on site.

AGENDA ITEM DESCRIPTION

Kyle Vrla, with Dynamic Engineering Consultants, applicant for the property owner, Monica Utter with Bill Utter Ford and Utter Properties, is requesting a change in zoning on approximately 18.5 acres. The rezoning request is for dual zoning. The intention is to rezone the current developed dealership (12.283 acres) from C-3, Commercial to Planned Development C-3 (PD C-3) and from C-2, Commercial to Planned Development C-2 (PD C-2) on approximately 6.2 acres adjacent to the existing dealership. The dealership intends to continue the existing New and Used Car / Pickup Truck Dealership with service included on the proposed PD C-3 developed site. Besides the Permitted Uses in C-3, Commercial found in the Unified Development Code (UDC) Use Chart; additional uses proposed are “Outside Storage of Inventory” and “Fuel Storage Tanks”.

The proposed PD C-2 zoning district of this site proposes uses as listed under the C-2, Commercial section of the Use Chart out of the UDC as well as “Automobile Dealership New and Used” and “Outside Storage of Inventory”.

Rezoning of the adjacent property owned by Utter Properties is a natural progression from the existing facility. The proposed planned development master plan includes construction of a carwash intended for use by the dealership only, expansion of the existing service shop and service drive as a part of the main building and a proposed expansion of the existing used car building. The additional acreage (6.2) which is proposed as PD C-2 will be an expansion of the parking lot and provide additional inventory parking.

SURROUNDING PROPERTIES ZONING

- Subject Property C-3, Commercial
- North C-1, Commercial
- Northeast PD C-2 (PD 33)
- Southeast C-2, Commercial
- West IH 35E Highway Commercial (PD-5) and C-2

SURROUNDING PROPERTIES EXISTING LAND USE

- Subject Property Commercial / Undeveloped
- North Office / Business Park
- Northeast Undeveloped
- Southeast Commercial
- West IH 35E Commercial

SURROUNDING PROPERTIES FUTURE LAND USE

- Subject Property Commercial / Undeveloped
- North Office / Business Park
- Northeast Commercial
- Southeast Commercial
- West IH 35E Commercial

PROPOSED ZONING

Planned Development C-3 on 12.283 acres proposing additional uses of “Outside Storage of Inventory” and “Fuel Storage Tanks” and Planned Development C-2 on 6.2 acres proposing additional uses of “Automobile Dealership New and Used” and “Outside Storage of Inventory”.

CONFORMANCE TO THE COMPREHENSIVE PLAN

The proposed zoning conforms to the existing Future Land Use designations of the City of Corinth Comprehensive Plan.

FINANCIAL SUMMARY

Source of Funding: No funding is required.

ATTACHMENTS / SUPPORTING DOCUMENTS

Location Map

Land Use Map

Zoning Map

Use Chart

Application Packet:

PD Letter of Intent

Design Statement and Concept Design Map with Proposed PD Standards and Regulations

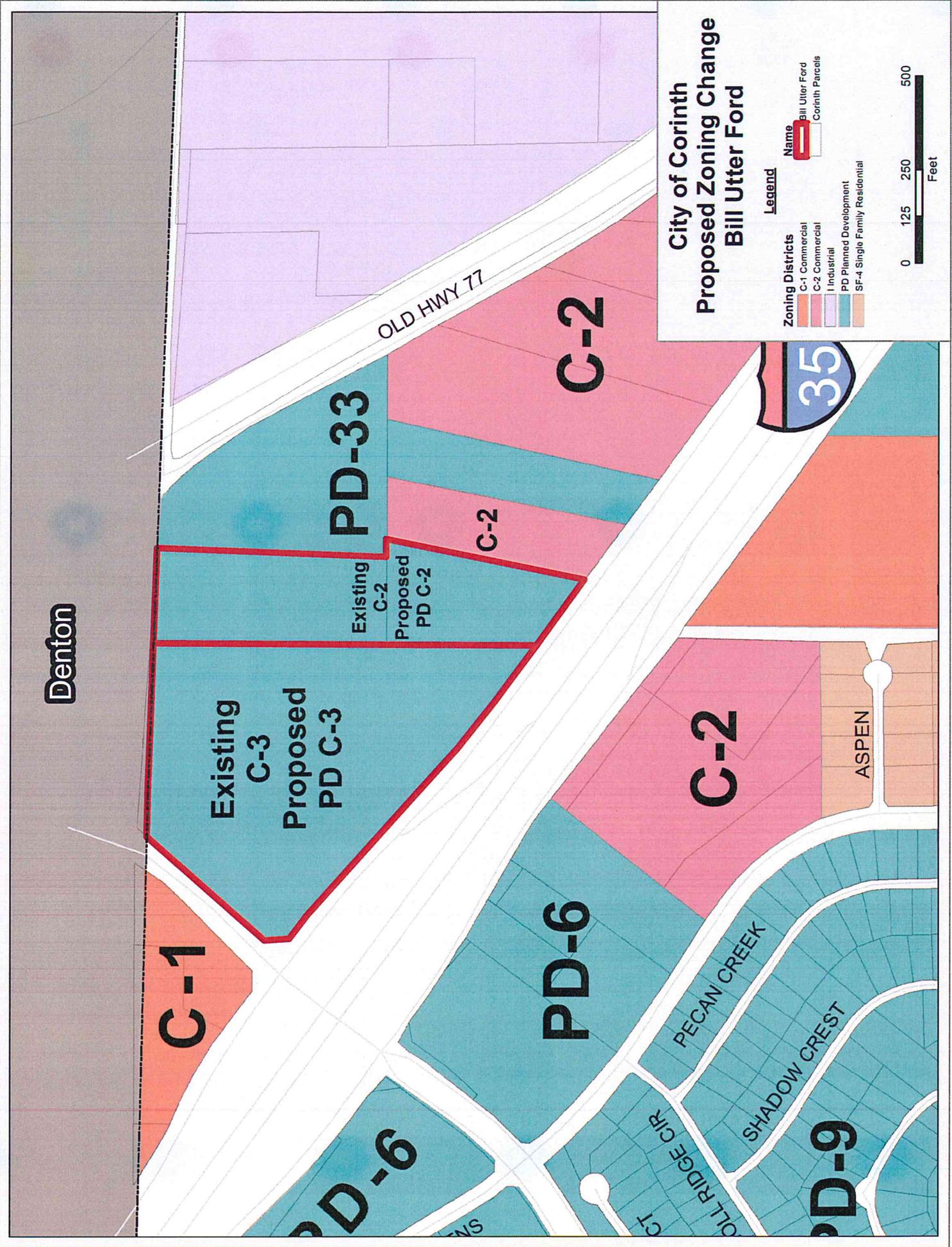
Elevations

Submitted By: Barbara Cabbage, Planning and Development Manager
Department: Planning and Development

Finance Review: Yes _ NA X

Legal Review: Yes _ NA _

Director Review and Approval:



Denton

C-1

Existing
C-3
Proposed
PD C-3

PD-33

Existing
C-2
Proposed
PD C-2

PD-6

PD-6

C-2

C-2

C-2

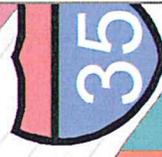
PECAN CREEK

SHADOW CREST

PD-9

ASPEN

OLD HWY 77



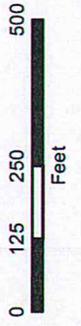
City of Corinth
Proposed Zoning Change
Bill Utter Ford

Legend

- Zoning Districts
- C-1 Commercial
 - C-2 Commercial
 - I Industrial
 - PD Planned Development
 - SF-4 Single Family Residential

Name

- Bill Utter Ford
- Corinth Parcels

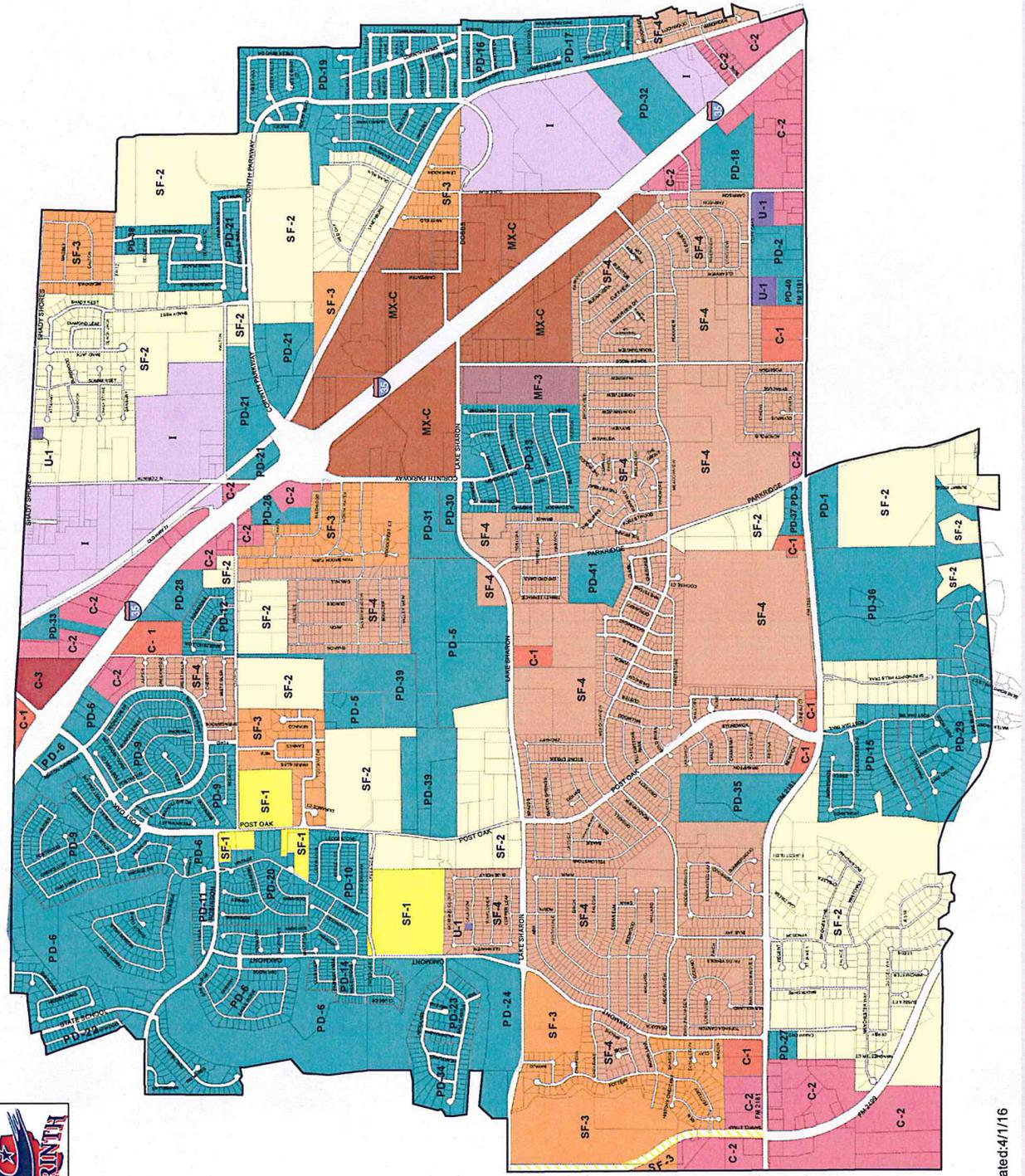
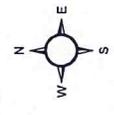




City of Corinth Zoning Map

Legend

- Zoning Districts**
- Future FM 2499
 - C-1 Commercial
 - C-2 Commercial
 - C-3 Commercial
 - I Industrial
 - MF-1 Multi-Family Residential
 - MF-2 Multi-Family Residential
 - MF-3 Multi-Family Residential
 - MHD Modular Home District
 - MX-C Mixed Use Commercial
 - PD Planned Development
 - SF-1 Single Family Residential
 - SF-2 Single Family Residential
 - SF-3 Single Family Residential
 - SF-4 Single Family Residential
 - U-1 Utility



Zoning Map Amendment (Rezoning) – Statement of Intent

Date: 03/01/2016

Project: Bill Utter Ford Expansion

Location: 4901 S. I-35E., Denton, TX 76210

Applicant: Dynamic Engineering Consultants, P.C.

The overall intent of this project is to expand the existing Bill Utter Ford dealership including expansion of the service center, body shop, used car sales, the addition of a stand-alone carwash facility and the expansion of inventory vehicle parking on the two adjacent properties to the East. The main lot with the existing dealership is currently zoned as C-3 and the two adjacent properties are currently zoned as C-2 and are currently undeveloped. The intent of the re-zoning is to zone all three lots as PD C-3. The planned development rezoning is to establish consistent regulations for all three properties as a whole with its future land use as automotive sales of new and used vehicles, body shop, carwash, and outside storage of inventory.

EXHIBIT B PD CONCEPT DESIGN STATEMENT

Statement of Purpose: The purpose of this PD ordinance is to allow for the development and expansion of the automobile dealership in operation at 4901 South I-35E in Corinth, Texas. The purpose of these general requirements and stipulations is to provide a framework for quality development consistent with City of Corinth zoning criteria and to provide consideration for existing properties adjacent to this property.

This planned development shall be called, Bill Utter Ford PD. The owners are developing the property and Monica Utter is the owner's representative. The site is generally located at the Southeast corner of I-35E and Lakeview Blvd. To the northwest, across Lakeview Blvd, the property is zoned as C-1 Commercial and currently used a Medical office center. Immediately to the north, exists a remnant piece of undeveloped land zoned as C-3 Commercial. To the northeast, exists an undeveloped property formerly zoned as C-2 and now zoned as PD-33. To the southeast, the existing site is zoned as C-2 Commercial and is currently developed as Caliber Collision body shop.

The site's existing use is an automobile dealership with sale of new and used vehicles along with a body shop. The proposed use includes the same automobile dealership with the sale of new and used vehicles along with a body shop, the addition of a carwash, aboveground fuel storage tanks, and outside storage of vehicle inventory. The proposed use fits the surrounding area and land usage and will not have a detrimental impact on the value. The proposed carwash is only for use by dealership personnel. It will not be open to the public, strictly for use by staff to clean customer's vehicles as a part of their service. Same with the aboveground fuel storage tank. Usage of the tank is strictly for staff to fill vehicles prior to sale and will not be open to the public. The outside storage of inventory is to better classify the large inventory parking on-site. Inventory parking may include double stack parking for additional storage space. Double stack parking means the vehicles will be "double parked." This is a typical practice for dealerships to increase inventory space without an additional drive aisle. These spaces are intended only for automobile inventory only and not for customer or employee usage. Two examples of double stack parking can be found at the Huffines Kia and Huffines Subaru directly across I-35E. Double stack parking for these examples can be found along the frontage road.

Proposed re-zoning of these parcels includes re-zoning the existing dealership property (12.283 acres) from C-3 Commercial to PD C-3. The adjoining parcels to the northeast (4.042 acres) and southeast (2.20 acres) will be re-zoned from C-2 Commercial to PD C-2. Only the land area located in the City of Corinth will be re-zoned. The small portion of property located in the City of Denton will remain as-is.

The proposed site layout includes an expansion of the existing service shop and service drive as a part of the main building. An expansion will be made to the used car building and the addition of a carwash facility will be included in the redevelopment. The parking lot will be expanded and configured to provide a more navigable and accessible lot.

There are no existing PD zoning districts in the development area however the northeast corner shares a property line with PD-33.

The selection of C-3 and C-2 Commercial districts have been selected as a base districts for the overall development. (See "Exhibit A" and "Exhibit C" for descriptions and a map of the proposed districts)

Special development regulations or modified regulations to the base zoning districts have been provided in "Exhibit D". Major waivers to the regulations listed in "Exhibit D" include the following:

- Addition of Automobile Dealership New and Used, Outside Storage of Inventory and Fuel Storage tanks to the allowed uses for the districts. (These are typical accessory uses with most dealerships and will not have a negative impact on the surrounding community.)
- Landscape reductions throughout the inventory parking area, corner regulations and along the frontage to increase visibility of the vehicles for sale.
- As an offset to the reductions, a landscape entry feature will be provided at the corner of the intersection of Lakeview Blvd and I-35E.
- Tree Preservation. Alternative compliance will be used to mitigate heavily wooded area at the south corner of the property. There are lots of existing trees on the site to be developed. The site layout has been re-configured multiple times to limit the number of trees affected. Most of the 2.20 acre property at the southwest corner of the development is remaining undeveloped to protect the existing trees in place.
- Pavement. Permeable Pavers to be used in a portion of the parking lot as a detention structure and water quality feature.
- Lighting. Existing lighting on the site may remain. Proposed lighting on the new addition will meet code. We have adjusted the regulation for existing lighting to come into effect with a 25% increase in building square footage. The cumulative proposed increase of building square footage calculates to a 24% increase, therefore the existing lights may remain.
Existing Cumulative Building Square Footage: 59,807 SF
Proposed Cumulative Building Square Footage: 78,605 SF
 $1 - (59,807 / 78,605) = 0.24$
- Parking. Customer parking spaces on the existing site will be re-striped to 10'x18'. Other existing parking will remain as-is. Proposed inventory storage parking spaces will be 9'x18'.
- Trash enclosure screening. Dumpsters/compactor will be within screened body shop parking area and will not have individual screening structures.
- Building façade. The building will be renovated to meet Ford's new Trusmark design. Modifications to the allowed building facade materials have been made to allow this renovation.

Existing streets around the development in question are:

- North (Lakeview Blvd.)
- West - I-35E
- East (Shady Shores Rd.)
- South (N/A)

No proposed public roads are proposed within this development.

The elevation of the existing site ranges from 630' to 585' above sea level. Existing slopes range from moderate to steep. A few areas on site exist at approximately a 12% slope. Proposed conditions provide approximately 6% slope in pavement areas and 3:1 slopes at tie in points (33%). Multiple trees exist on the site including a heavily wooded area on the southern portion of the site. The proposed improvements have been designed in such a way to avoid the most dense area of trees and allow them to remain. The existing and proposed sites generally drain to the southeast. A small portion of the existing site drains to Lakeview Blvd and to I-35E.

The existing site drains in three general directions. The northern portion of the site (4.19 acres) drains to the north toward Lakeview Blvd and Shady Shores Road where the water is collected into existing underground storm sewers. A small portion of the site (0.4 acres) along the western property line drains toward I-35E. The flow is collected in TxDOT's existing drainage ditch parallel to the frontage road and routed to the south toward an existing drainage channel. A large portion of the site (13.6 acres) drains toward the southeast and eventually flows into an existing drainage channel that runs toward the east

toward Old US Highway No. 77. An existing TxDOT culvert exists at the southwest corner of the site. The culvert is currently a 3-30" RCP but will be replaced with 2-6'x4' RBC per the Phase 1 I-35E plans from TxDOT. The culvert will be replaced in place. There are no FEMA flood plains within the proposed development.

The proposed development maintains the existing drainage patterns and flows. A large area of permeable pavers and associated aggregate bedding will be installed on the downhill side along the eastern property line. The pavers will be used not only to collect and route the water, they will be used as an underground detention pond and permanent water quality feature on-site. The pavers will be on a rock structure base approximately 27" in depth. At a void ratio of 40%, the required detention volume will be met. The proposed outfall will be located along the existing drainage channel. Locating the outfall at this specific location improves the existing conditions of full site drainage flowing to the adjacent property before making its way into the existing channel.

Proposed public utility lines include an 8" fire loop with two proposed fire hydrants within a proposed 16' water easement. Per the request of the Fire Department, two fire hydrants have been provided in the vehicle inventory parking area as well as a fire lane. One private 2" service will be installed for the proposed carwash facility. Additionally, one private 4" sanitary sewer service will be installed with a sand/oil separator to serve the carwash and shop expansion. All other utility services are existing and will use existing connections for the building expansions.

As mentioned before, this project will be expanding the existing service bay, service shop, used car sales, adding a car wash facility, and expanding the outside vehicle inventory storage lot. All proposed structures will match the existing structures in size and appearance. The showroom will also receive a facelift renovation to keep with the current Ford Trustmark design theme as seen at many other new or renovated Ford dealerships.

EXHIBIT "C"
PD CONCEPT DESIGN MAP

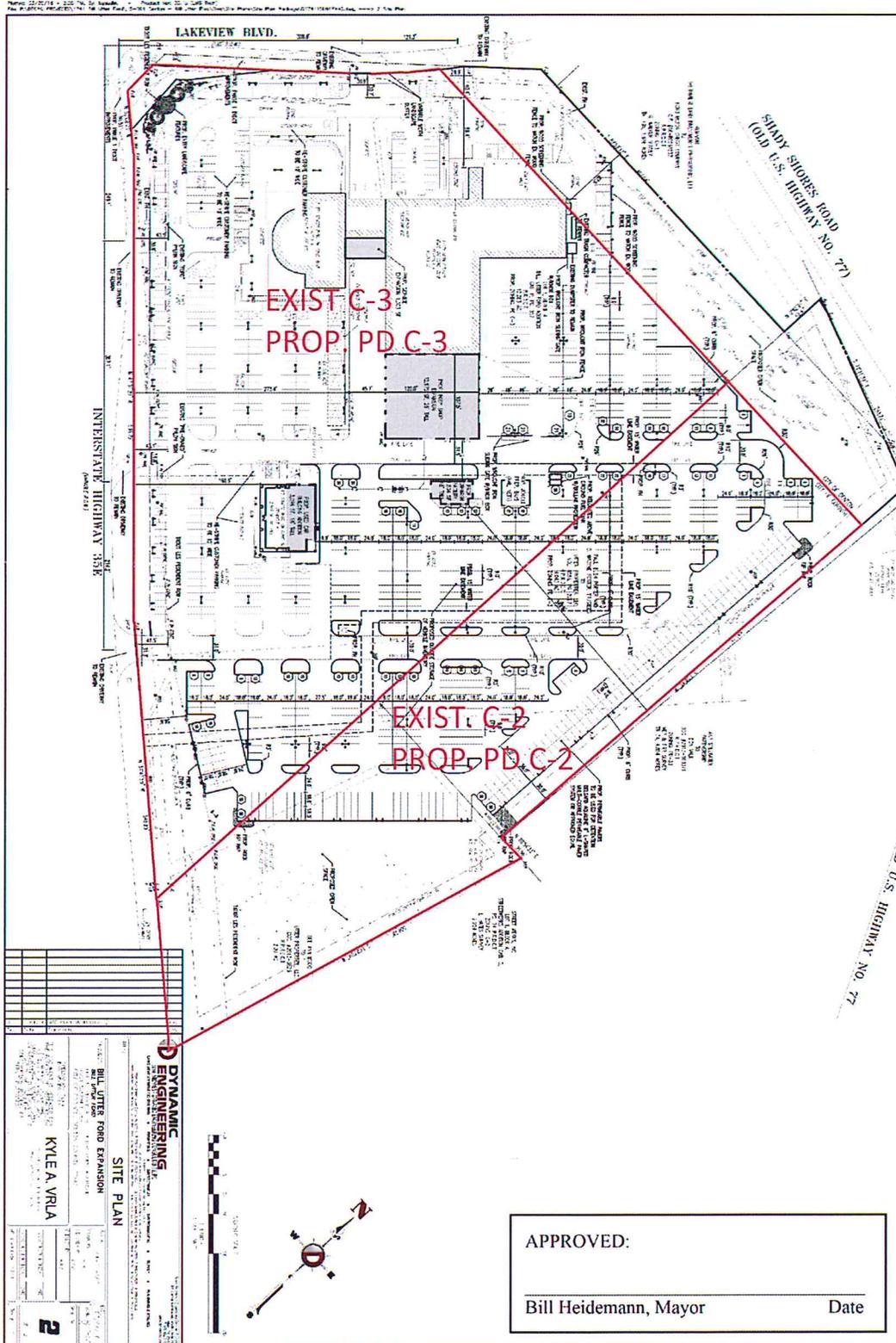


EXHIBIT “D” LAND USE REGULATIONS

SECTION 1: REGULATIONS

A. Purpose

The regulations set forth provide development standards for Planned Development C-3, Commercial District. The Planned Development (PD) District is identified by metes and bounds called out in Exhibit A and is depicted on Exhibit B. Every use not authorized herein is expressly prohibited in this Planned Development (PD) District.

B. Base Districts

C-3, Commercial District, regulations of the Corinth Unified Development Code, Ordinance No. 13-05-02-08 shall apply except as modified herein.

SECTION 2: USES

A. C-3 Commercial District

In the Planned Development (PD) District Uses, no building, or land shall be used and no building shall be hereafter erected, reconstructed, enlarged, or converted unless otherwise provided in the C-3, Commercial District, regulations of the Unified Development Code.

SECTION 3: AREA REGULATIONS

A. Purpose

This district is intended to provide for expansion of the existing THE BILL UTTER FORD NEW AND USED CAR / PICKUP TRUCK DEALERSHIP

B. Permitted Uses and Use Regulations

The Permitted Uses in the C-3 and C-2, Commercial Districts, as listed in Subsection 2.07 of the Unified Development Code, shall be permitted uses in the areas defined in “Exhibit B” and “Exhibit C” and each shall include the following uses, respectively.

PD C-3

1. Outside Storage of Inventory (accessory)
2. Fuel Storage Tanks (accessory)

PD C-2

1. Automobile Dealership New and Used (main)
2. Outside Storage of Inventory (accessory)

C. Dimensional Regulations

The Dimensional Regulations described in Section 2.08 of the Unified Development Code, Ordinance No. 13-05-02-08, for the C-3 and C-2, Commercial Districts, shall apply to the development of the property, except as follows:

1.

| | Current C-3 | Current C-2 | Proposed PD C-3 | Proposed PD C-2 |
|---|--------------------|--------------------|------------------------|------------------------|
| Minimum Front Yard Setback: | 40 | 40 | 40 | 40 |
| Minimum Side Yard Setback: Interior Lot | 20 | 0 | 20 or 0* | 0 |
| Minimum Side Yard Setback: Corner Lot | 40 | 10 | 40 or 0* | 10 or 0* |
| Minimum Rear Yard Setback: | 20 | 20 | 20 or 0* | 20 or 0* |
| Minimum Lot Area | 40,000 | 30,000 | 40,000 | 30,000 |
| Minimum Lot Width | 200 | 175 | 200 | 175 |
| Minimum Lot Depth: | 150 | 120 | 150 | 150 |
| Maximum Height | 40 | 40 | 40 | 40 |
| Maximum Building Area – all buildings | 50% | 50% | 50% | 50% |

* No setback required along shared zoning line within this PD. Setbacks only required on exterior property/zoning boundaries adjacent to other districts not included in this PD.

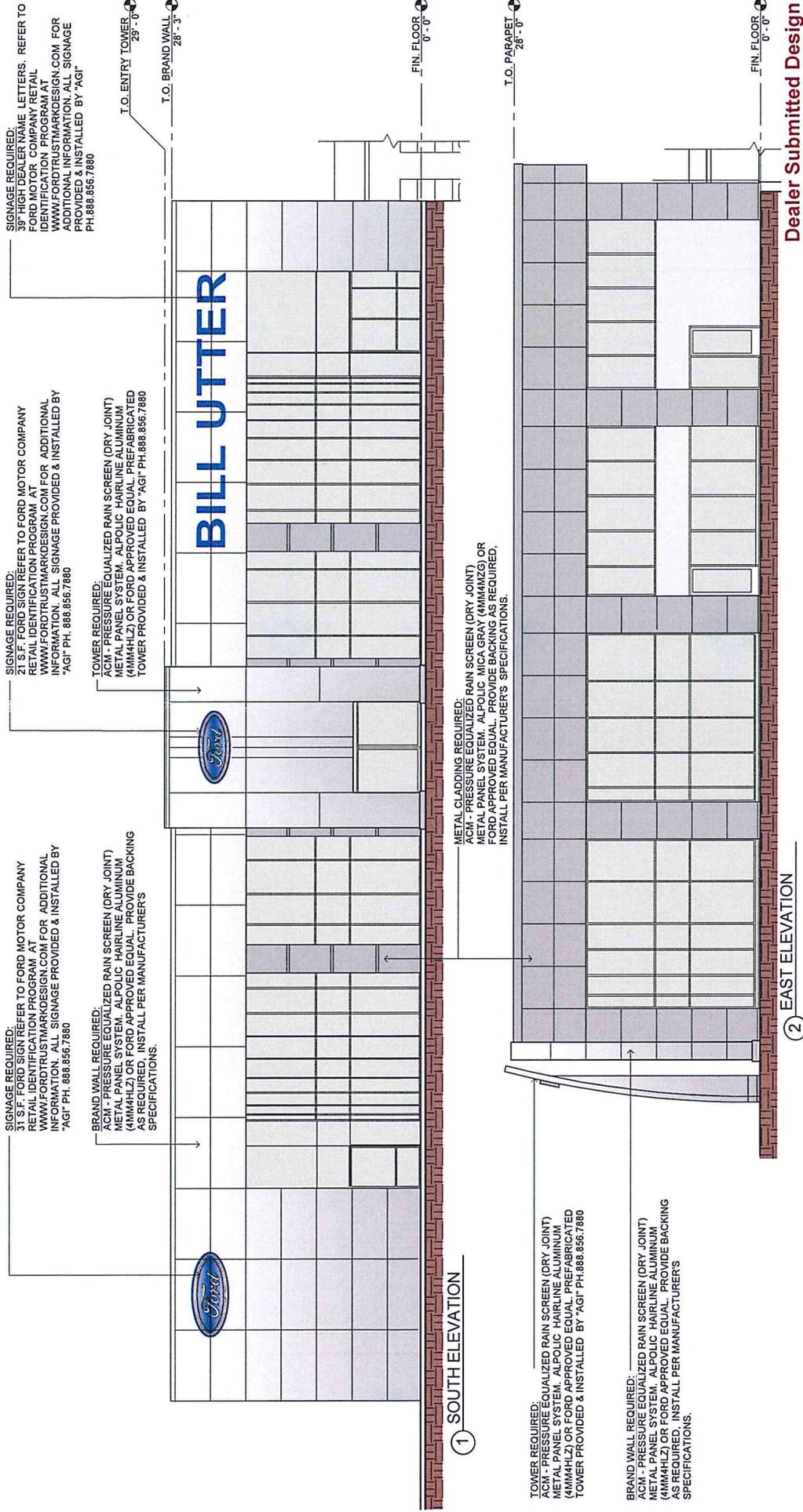
D. Development Standards

The Development Standards for this PD are the development standards for both C-3 and C-2, Commercial Districts, Section 2.04.07 of the City’s Unified Development Code except as otherwise stated herein.

1. UDC 2.07.07 **Accessory Buildings and Uses** shall apply except:
 - a. One (1) stand-alone carwash building is allowed.
 - b. Permanent hail nets are allowed throughout the site.
 - (i) Hail nets shall cover no more than a maximum of 40% of vehicle parking spaces.
 - (ii) Hail nets shall not conflict with fire protection and fire apparatus accessibility.
 - (iii) Foundations for hail nets may not be constructed within easements.

1. Spanning of the hail nets over easements lines is allowed with approval from the easement grantee.
2. UDC 2.09.01 **Landscape Regulations** shall apply except:
 - a. Existing landscape buffer dimensions shall remain in place along the frontages within the area of land known as Lot 1 Block A, Bill Utter Ford Addition.
 - (i) A twenty (20) landscape buffer exists along I-35E.
 - (ii) Due to previous R.O.W. take for the expansion of Lakeview Blvd, a variable width (19' to 7') landscape buffer exists along Lakeview Blvd.
 - (iii) The property will not be required to meet any specific landscape buffer with any future R.O.W. takes along both I-35E and Lakeview Blvd.
 - b. New development along I-35E shall be set back a minimum of 55' from the existing R.O.W. line in anticipation for R.O.W. expansion.
 - (i) The property will not be required to meet any specific landscape buffer with any future R.O.W. take along I-35E.
 - (ii) Shrubs will be provided in lieu of required trees along I-35E in areas of new development to provide frontage visibility of the vehicles for sale.
 - c. Existing landscaping throughout existing site where no improvements are occurring may remain as-is and not be required to be improved.
 - d. There shall be ten (10) square feet of interior Parking Lot Landscaping for each parking space or fraction thereof.
 - e. There shall be one (1) shade tree (3" caliper minimum) or an ornamental tree for every twenty (20) spaces or fraction thereof.
 - f. To offset the above reductions in landscaping, a landscape feature shall be provided at the property corner near the intersection of I-35E and Lakeview Blvd.
 - (i) The landscape and display feature must include a variety of plantings, shrubs, and ornamental trees.
 - (ii) Flag poles at a maximum height of 40' may be allowed as a part of this feature.
3. UDC 2.09.02 **Tree Preservation Regulations** shall except:
4. UDC 2.09.03 **Vehicle Parking Regulations** shall apply except:
 - a. Existing parking spaces on site may remain in their current configuration and size.
 - (i) All inventory parking spaces will be 9' x 18'.
 - (ii) Existing two (2) Electric Vehicle (EV) spaces on site will suffice for EV requirement. Inventory parking spaces shall not apply to EV requirement.
 - b. Three (3) stacking spaces shall be provided per bay for the Car Wash.

5. UDC 2.09.04 **Building Façade Material Standards** shall apply except:
 - a. Exterior materials for the proposed building and future development may include, but not limited to, architectural metal panels in flat, smooth, flat seamed, ribbed and s panel configurations, EFIS, brick, CMU or natural stone masonry, glass storefront and steel canopies. Other materials as allowed by City of Corinth ordinances shall be acceptable.
 - b. Existing building façade may remain as-is for portion of the existing building not being renovated.
6. UDC 2.09.05 **Residential Adjacency Standards** shall apply.
7. UDC 2.09.06 **Nonresidential Architectural Standards** shall apply.
8. UDC 2.09.07 **Lighting and Glare Regulations** shall apply except:
 - a. If a person makes any change or addition to the existing buildings that result in a cumulative increase in the size of the buildings by more than twenty-five (25) percent, the person shall ensure that the existing outdoor lighting shall conform to the provisions of this UDC.
9. UDC 4.01 **Sign Regulations** shall apply except:
 - a. Existing signage may remain in place and operational on site.
10. UDC 4.02 **Fence and Screening Regulations** shall apply except:
 - a. Existing pipe rail fencing may remain around perimeter of site to provide vehicle restraint as a security measure.
 - b. Additional pipe rail fencing around perimeter shall match existing fence.
 - c. Screening fence around body shop area may be wood fence to match existing screening fence.
 - (i) Dumpsters located within screened fence area (i.e. body shop) will not require individual screening enclosures.



Dealer Submitted Design



LOCAL ARCHITECT TO V.I.F. ALL EXISTING BUILDING CONDITIONS.

For communication of design intent only. Local architect must review for code compliance. These drawings are not suited or intended for construction or fabrication. All contents on this sheet are confidential. Information and all rights therein are and will remain the property of Ford Motor Company.



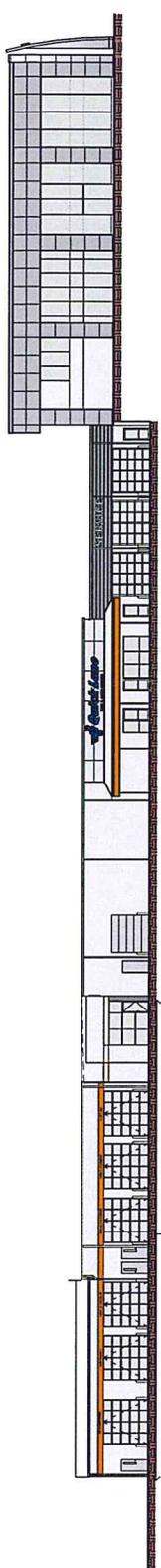
ADDITIONAL PROTOTYPICAL DRAWINGS AND DETAILS FOR THE FORD TRUSTMARK FACILITY DESIGN CAN BE FOUND AT WWW.FORDTRUSTMARKDESIGN.COM

Date: 12.08.2015
 City / State: Denton, TX

A 601



Bill Utter Ford Proposed Exterior Elevations



① OVERALL WEST ELEVATION
1/32" = 1'-0"

QUICK LANE BRANDING ELEMENTS REQUIRED:
REFER TO QUICK LANE PRE-PLAN DATED 10/18/12

TOWER REQUIRED:
ACM - PRESSURE EQUALIZED RAIN SCREEN (DRY JOINT) METAL PANEL SYSTEM. ALPOLIC HAIRLINE ALUMINUM (4MM4HLZ) OR FORD APPROVED EQUAL. PREFABRICATED TOWER PROVIDED & INSTALLED BY "AGI" PH.888.856.7880

BRAND WALL REQUIRED:
ACM - PRESSURE EQUALIZED RAIN SCREEN (DRY JOINT) METAL PANEL SYSTEM. ALPOLIC HAIRLINE ALUMINUM (4MM4HLZ) OR FORD APPROVED EQUAL. PROVIDE BACKING AS REQUIRED. INSTALL PER MANUFACTURER'S SPECIFICATIONS.

METAL CLADDING REQUIRED:
RIBBED METAL PANELS (EXPOSED, PAINTED FASTENERY, ALCOA SLATE GREY OR FORD APPROVED EQUAL. REFER TO WWW.FORDTRUSTMARKDESIGN.COM FOR SPECIFICATIONS

SIGNAGE REQUIRED:
26" HIGH SERVICE & PARTS LETTERS. REFER TO FORD MOTOR COMPANY RETAIL IDENTIFICATION PROGRAM AT WWW.FORDTRUSTMARKDESIGN.COM FOR ADDITIONAL INFORMATION. ALL SIGNAGE PROVIDED & INSTALLED BY "AGI" PH.888.856.7880

SIGNAGE REQUIRED:
QUICK LANE SIGNAGE. REFER TO FORD MOTOR COMPANY RETAIL IDENTIFICATION PROGRAM AT WWW.FORDTRUSTMARKDESIGN.COM FOR ADDITIONAL INFORMATION. SIGNAGE PROVIDED & INSTALLED BY "AGI" PH.888.856.7880

BRAND WALL REQUIRED:
ACM - PRESSURE EQUALIZED RAIN SCREEN (DRY JOINT) METAL PANEL SYSTEM. ALPOLIC HAIRLINE ALUMINUM (4MM4HLZ) OR FORD APPROVED EQUAL. PROVIDE BACKING AS REQUIRED. INSTALL PER MANUFACTURER'S SPECIFICATIONS.

EXTERIOR PAINT REQUIRED:
COLOR: UNIVERSAL GREY, OCONN 627 000 ORDER# A2004
MANUFACTURER: GLIDDEN PROFESSIONAL

EXISTING GLAZING TO REMAIN

GLAZING SYSTEM REQUIRED:
CLEAR ANODIZED ALUMINUM CURTAINWALL - OR STOREFRONT SYSTEM

METAL CLADDING REQUIRED:
ACM - PRESSURE EQUALIZED RAIN SCREEN (DRY JOINT) METAL PANEL SYSTEM. ALPOLIC MICA GRAY (4MM4MZG) OR FORD APPROVED EQUAL. PROVIDE BACKING AS REQUIRED. INSTALL PER MANUFACTURER'S SPECIFICATIONS.

② ENLARGED WEST ELEVATION - SERVICE / PARTS & SHOWROOM
1/16" = 1'-0"

Bill Utter Ford

Proposed Exterior Elevations - Service

Dealer Submitted Design



LOCAL ARCHITECT TO V.I.F. ALL EXISTING BUILDING CONDITIONS.

For communication of design intent only. Local architect must review for code compliance. These drawings are not suited or intended for construction or fabrication. All contents on this sheet are confidential. Information and all rights therein are and will remain the property of Ford Motor Company.



ADDITIONAL PROTOTYPICAL DRAWINGS AND DETAILS FOR THE FORD TRUSTMARK FACILITY DESIGN CAN BE FOUND AT WWW.FORDTRUSTMARKDESIGN.COM

Date: 12.08.2015
City / State: Denton, TX

Scale: AS NOTED

A 602



PUBLIC HEARING/BUSINESS AGENDA
ITEM #10 and 10a.
Planning and Zoning Commission Special Session
April 18, 2016

AGENDA ITEM

PUBLIC HEARING: TO HEAR PUBLIC OPINION REGARDING A REQUEST FROM THE APPLICANT JOHN THATCHER, AUTHORIZED REPRESENTATIVE FOR THE PROPERTY OWNERS, TOMMY JOSEPH ROBINSON, BRIAN KEITH ROBINSON, RAYMON MANN ROBINSON, BRANDY DENISE ROBINSON, ROBERT MANLEY ROBINSON, THOMAS MANN ROBINSON, AND TIMOTHY MANLEY ROBINSON FOR A ZONING CHANGE FROM C-1, COMMERCIAL TO PLANNED DEVELOPMENT C-2 COMMERCIAL WITH MODIFIED DEVELOPMENT STANDARDS ON 10.603 ACRES OF LAND LEGALLY DESCRIBED AS LOT 1, BLOCK A, TRI-STEEL ADDITION, IN THE CITY OF CORINTH, DENTON COUNTY, TEXAS.

BUSINESS: Consider and act on a zoning change from C-1, Commercial to Planned Development C-2 on 10.603 acres of land legally described as Lot 1, Block A, Tri-Steel Addition, in the City of Corinth, Denton County, Texas. (This property is located on the south side of I-35E, between Post Oak Drive and Church Drive.)

APPROVAL PROCESS

A public hearing will be held for the rezoning request. The rezoning request is in conformance with the Comprehensive Plan. Therefore, no amendment to the Comprehensive Plan is required in order for the Planning and Zoning Commission to consider this request.

However, the proposed automobile and truck sales facility, new and/or pre-owned (used) is **not** currently allowed in a C-1 Commercial zoning district and is **only** allowed in C-2 Commercial district **upon approval of an SUP with conditions**. Please see the following chart below listing the allowable uses by the C-1 and C-2 zoning districts for all of the proposed uses, including the accessory uses for this request.

| Proposed Non-Residential Uses | C-1 | C-2 |
|--|-------------------------|-------------------------|
| Automobile and Truck Sales Facility, new and/or pre-owned (used) | Not Allowed | SUP with Conditions |
| Automobile Parts Store | Allowed | Allowed |
| Automobile Service Garage (Minor) | Allowed | Allowed |
| Outside Display | Allowed with Conditions | Allowed with Conditions |
| Outside Storage | SUP with Conditions | SUP with Conditions |
| Gasoline Filling/Car Wash | Not Allowed | SUP with Conditions |

The recommendation of the Planning and Zoning Commission for this rezoning request will be forwarded to the City Council for final action at the May 5, 2016, City Council regular session meeting.

AGENDA ITEM DESCRIPTION

The applicant is proposing a rezoning from C-1 Commercial on 10.603 acres, to a Planned Development C-2 Commercial with modified development standards in order to develop a Carmax, automobile and truck sales facility, new and/or pre-owned (used) and accessory uses, such as new auto sales parts (tires and accessories), automobile service garage for routine services and minor repairs to be conducted inside the building, one (1) non-public car wash building, and an underground fuel storage area with one (1) fuel pump.

The applicant is proposing the following in lieu of the C-2 dimensional regulations per Section 2.08.05 of the UDC as shown in the chart below.

| C-2 | REQUIRED | PROVIDED |
|-----------------------------|----------|----------|
| Minimum Side Yard Setbacks: | | |
| Interior Lots: | | |
| East | 0' feet | 15' feet |
| West | 15' feet | 25' feet |
| Minimum Rear Yard Setback | 20' feet | 50' feet |

The applicant is meeting or exceeding all other requirements of the UDC, except the following:

1. UDC 2.09.01 **Landscape Regulations** shall apply except:
 - a. Interior parking lot landscaping in the outdoor automobile sales display area or the automobile sales staging area shall not be required. The interior parking lot trees that would be required in the sales display/staging area shall be planted and maintained in the residential adjacency area landscape edge buffer yards along the west and south property lines, and along I-35E as shown on the Design Map/Concept Plan, as shade trees with a minimum 3” caliper size (per Table 15: Approved Plant Material List). Additionally, parking lot trees per 3” caliper minimum (per Table 15: Approved Plant Material List) shall be planted per the Interior Parking Lot Landscaping requirements.
 - b. A 50’ wide tree preservation area shall be established along the south property line to provide an increased landscape buffer to the adjacent single family residential

property, with the provision that limited drainage improvements shall be provided within this area as needed.

- c. In addition to the required landscaping/screening for parking lots adjacent to residential areas, a 15' wide tree preservation area shall be established and maintained along the west property line.
2. UDC Section 2.09.002 **Tree Preservation** shall apply except:
 - a. A 50' wide tree preservation area shall be established along the south property line to provide an increased landscape buffer to the adjacent single family residential, with the provision that drainage requirements are met.
 - b. A 15' wide tree preservation area shall be established along the west property line.
 3. UDC 2.09.03 **Vehicle Parking Regulations** shall apply except:
 - a. Parking space dimensions for sales display may be 9'x17' with 20' wide maneuvering/drive aisle lanes only.
 4. UDC Section 2.09.07 **Lighting and Glare Regulations** shall apply except:
 - (a) Required Turning Off of Property Luminaires
 - (i) All luminaires within the Property, except those required for security, shall be extinguished within one hour after the end of business hours and remain extinguished until one hour prior to the commencement of business hours. Normal business hours shall be similar to other Texas CarMax locations which are 9:00 AM to 9:00 PM.
 - (ii) A maximum average level of five foot-candles at building and gated parking lot entrances is permitted and a maximum level of one foot-candle on the rest of the structure. A maximum level of two foot-candles is permitted along the staging and preparation area security wall in the southwest portion of the site. The site shall comply with the shielding requirements of Section 2.09.07.C.1. so that substantially all the directly emitted luminous flux falls within the property line.
 5. UDC 4.02 **Fence and Screening Regulations** shall apply except:
 - a. The masonry screening wall separating nonresidential used and/or zoned areas from residential uses as required under Section 4.02.11 shall be eight (8') feet in height and shall be placed at the edge of the parking areas on the south and west sides in the general locations shown on the PD Concept Design Map, instead of at the property line, to provide for the above described Tree Preservation Areas.

NOTIFICATION TO PUBLIC

Prior to the Planning and Zoning Commission meeting, public hearing notifications were sent to the 34 property owners located within 200' of the subject property. A notice of public hearing was posted on the subject property along I-35E.

SURROUNDING PROPERTIES ZONING

- Subject Property C-1, Commercial-1
- North N/A (I-35E)
- South PD (SF-1), Single Family Residential
- East PD (C-2), Commercial-2
- West SF-4, Single Family Residential/C-2, Commercial

SURROUNDING PROPERTIES EXISTING LAND USE

- Subject Property Vacant Commercial (Demolition in Progress)
- North N/A (I-35E)
- South Single Family, Residential
- East Undeveloped/Motor Vehicle Sales
- West Single Family, Residential/Motor Vehicle Sales

SURROUNDING PROPERTIES FUTURE LAND USE

- Subject Property Commercial
- North Commercial
- South Low Density Residential
- East Commercial
- West Low Density Residential/Commercial

PROPOSED LAND USE

The applicant is proposing auto sales and service with accessory uses in order to develop a Carmax facility.

CONFORMANCE TO THE COMPREHENSIVE PLAN

The Unified Development Code requires consistency between a Zoning Map Amendment (Rezoning) and the Comprehensive Plan. The proposed zoning is in conformance with the Comprehensive Plan. Therefore, a Comprehensive Plan Amendment is not necessary.

FINANCIAL SUMMARY

Source of Funding: No funding is required.

STAFF RECOMMENDATION

Staff does **Not** recommend **Approval** of this request for the following reasons:

1). The proposed use is too intensive for this location, as it is surrounded on the south and west by single family residentially zoned and developed properties. 2). There are already a large number of motor vehicle sales along this major I-35E corridor within the City of Corinth. 3). There are limited available parcels remaining along this major I-35E corridor within the City of Corinth for retail, corporate campus office, hospitality, medical and entertainment opportunities which market studies show this area can support. 4). I-35E is a major corridor within the City of Corinth that provides excellent visibility and access making it attractive for future retail, office, medical and entertainment development. 5). Retail development is crucial to a healthy tax base. 6). Neighborhood services, such as retail, restaurant, employment and recreational/entertainment opportunities **are appropriate** in this location and are vital in providing for the health, and welfare of our current and future residents and families within the City of Corinth.

ATTACHMENTS/SUPPORTING DOCUMENTS

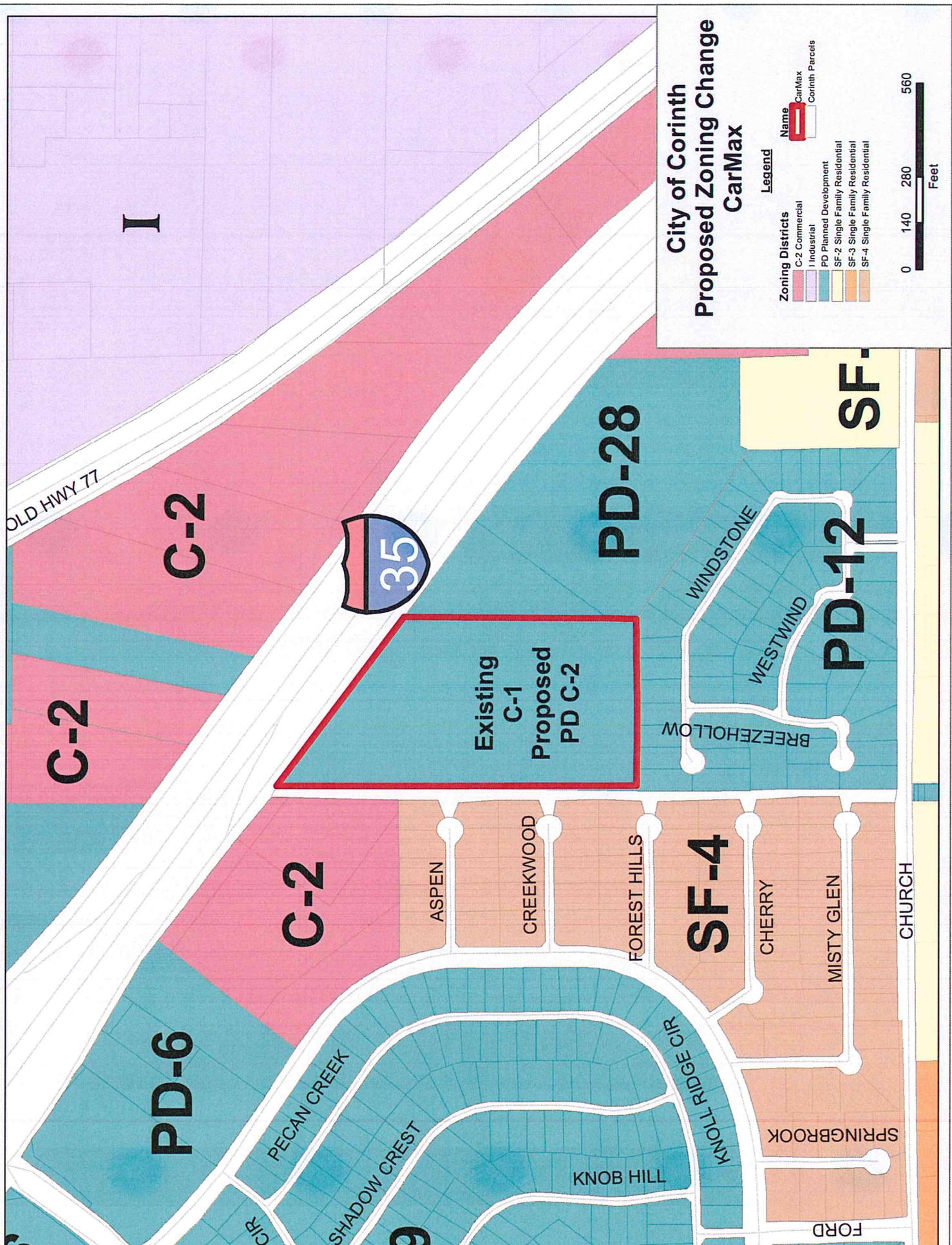
Location Map
Letter of Intent/PD Design Statement
PD Design Map/Concept Plan
PD Land Use Regulations

Submitted By: Lori Levy, AICP
Department: Planning and Development

Finance Review: Yes ___ NA X

Legal Review: Yes ___ NA X

Director Review and Approval:



City of Corinth Proposed Zoning Change CarMax

Legend

| Zoning Districts | Name |
|--------------------------------|-----------------|
| C-2 Commercial | CarMax |
| I Industrial | Corinth Parcels |
| PD Planned Development | |
| SF-2 Single Family Residential | |
| SF-3 Single Family Residential | |
| SF-4 Single Family Residential | |



Existing
C-1
Proposed
PD C-2

OLD HWY 77



PD-28

SF-

PD-12

BREEZEHOLLOW
WESTWIND
WINDSTONE

C-2

C-2

I

C-2

ASPEN

CREEKWOOD

FOREST HILLS

SF-4

CHERRY

MISTY GLEN

CHURCH

PD-6

PECAN CREEK

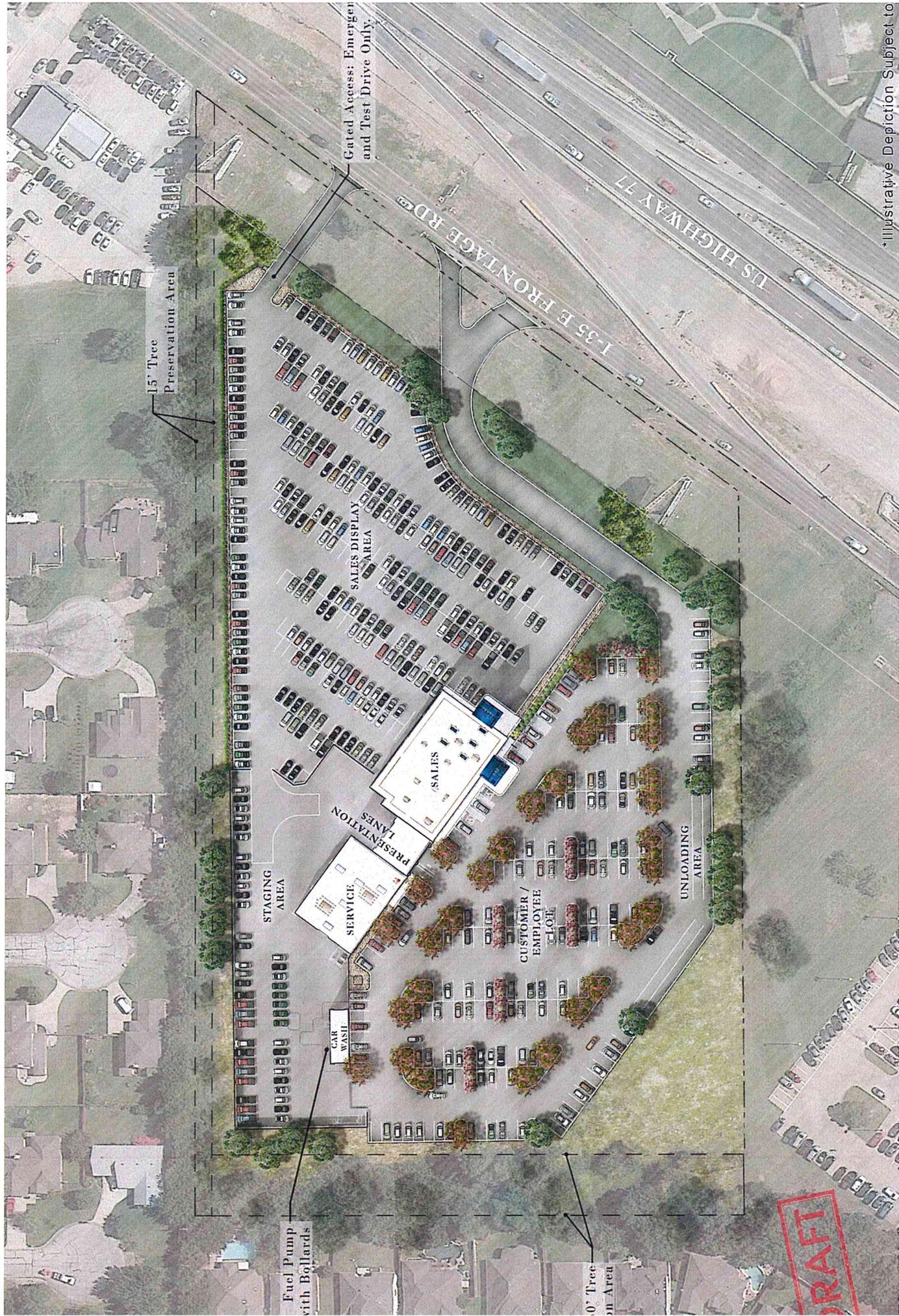
SHADOW CREST

NOB HILL

KNOLL RIDGE CIR

SPRINGBROOK

FORD



* Illustrative Depiction Subject to



CORINTH, TX
April 2016



Prepared by:
 SES Group Inc. www.sesgroup.com
 1201 14th Street, Suite 200
 Denver, CO 80202
 Phone: 303.531.4925

Prepared for:
 CenterPoint Integrated Solutions
 1240 Began Parkway Street A-250
 Englewood, CO 80155

Concept Design Map

PUBLIC HEARING / BUSINESS ITEM # 11 & 11a

Planning and Zoning Commission Special Session

April 18, 2016

AGENDA ITEM

- 11. PUBLIC HEARING:** TO HEAR PUBLIC OPINION REGARDING A REQUEST FROM THE APPLICANT RAY HUFFINES AUTHORIZED REPRESENTATIVE FOR DWIGHT WALKER, MANAGING GENERAL PARTNER FOR THE PROPERTY OWNERS DOROTHY WILLIAMS FAMILY PS LTD AND R. O. DUNCAN FOR AN AMENDMENT TO THE COMPREHENSIVE PLAN FUTURE LAND USE DESIGNATION FROM MIXED USE NON-RESIDENTIAL TO COMMERCIAL ON 13.773 ACRES OF LAND SITUATED IN THE E. A. GARRISON SURVEY, COUNTY ABSTRACT 511 BEING TRACTS 36, 35, 34, 33, AND 24 IN THE CITY OF CORINTH, DENTON COUNTY, TEXAS.
- 11a. BUSINESS:** Consider and act on an amendment to the Comprehensive Plan Future Land Use designation from Mixed Use Non-Residential to Commercial on 13.773 acres of land situated in the E. A. Garrison Survey, County Abstract 511 being tracts 36, 35, 34, 33, and 24 in the City of Corinth, Denton County, Texas.

APPROVAL PROCESS

Public hearings will be held and a recommendation from the Planning and Zoning Commission will be required to be presented to City Council. This **requested Comprehensive Plan Land Use Amendment** as well as the applicant's proposed zoning change on this site are intertwined and it would be virtually impossible to discuss one without consideration of the other. However, when it is time for the Commission to make a recommendation; the Comprehensive Plan Amendment must be acted on first per the UDC requirements that states a Zoning Map Amendment (Rezoning) must be consistent with the Comprehensive Plan.

The recommendation of the Planning and Zoning Commission will be forwarded to the City Council for final action at the May 5, 2016 City Council regular session meeting.

STAFF RECOMMENDATION

Staff recommends denial of the proposed Amendment to the Comprehensive Plan Land Use Designation. The Comprehensive Plan and the proposed change does not portray the vision of the Steering Committee as presented in the Comprehensive Plan. Staff is not supportive of this change as it is not an appropriate change or a good fit for this area.

However, should the Amendment to the Comprehensive Plan be recommended for approval as well as the rezoning request, Staff would recommend that the City consider amending the Land Use designation of the adjoining properties as well.

AGENDA ITEM DESCRIPTION

The Applicant is requesting an Amendment to the Comprehensive Plan from Mixed Use Non-Residential to Commercial. The Applicant is also requesting a zoning change from MX-C, Mixed Use Commercial to Planned Development MX-C. This proposed development includes land development and construction of

new (KIA and Subaru) automobile dealerships, with service areas, a used car facility and an auto body shop. This proposed development would cover more than 13 acres of land with the dealerships and service entrances fronting on IH 35E and the auto body shop would be located towards the back side of the property fronting on Tower Ridge Road. With this zoning change request, if approved, an amendment to the Comprehensive Plan would be required.

SURROUNDING PROPERTIES ZONING

- **Subject Property** **MX-C, Mixed Use Commercial**
- North MX-C, Mixed Use Commercial
- South MX-C, Mixed Use Commercial
- East MX-C, Mixed Use Commercial with IH 35E divider
- West MF-3, Multi-Family Residential

SURROUNDING PROPERTIES EXISTING LAND USE

- **Subject Property** **Undeveloped**
- North Residential
- South Commercial – Destiny Dallas
- East Low Density Residential / Undeveloped / Office with IH 35E divider
- West Low Density Residential / High Density Residential

SURROUNDING PROPERTIES FUTURE LAND USE

- **Subject Property** **Mixed Use Non-Residential**
- North Mixed Use Non-Residential
- South Mixed Use Non-Residential
- East Mixed Use Non-Residential with IH 35E divider
- West High Density Residential

PROPOSED LAND USE AMENDMENT

Application has been made to amend the Future Land Use Designation and Map of Corinth's Comprehensive Plan from Mixed Use Non-Residential to Commercial on 13.773 acres.

PROPOSED PROJECT

- The Current Land Use – Mixed Use Non-Residential does not support these uses - “Automobile Sales and Service” and “Auto Body Shop”. The Comprehensive Plan Future Land Use category for Mixed Use Non-Residential specifically calls out this area to have an emphasis on design and appearance. Public Spaces, with design regulations and that should be strictly enforced to ensure a positive visual perception along IH 3E.
- The Proposed Land Use – Commercial is more appropriate for these uses; “Automobile Sales and Service” and “Auto Body Shop”. The Commercial Land Use category which encompasses the automobile-related services industry includes more outside storage which is described in this Land Use designation as being an area that would tend to lessen the visual perception of such roadways.

- The Current Land Use includes an element of residential. Although the Base District MX-C allows for this use; the PD would eliminate that residential element as it is not an allowed element in the Proposed Land Use – Commercial.
- The subject property is currently zoned MX-C, Mixed Use Commercial.
- The proposed Zoning is Planned Development MX-C. The uses, “Automobile Sales and Service” use would require approval of a Specific Use Permit but “Auto Body Shop” would not be allowed in MX-C zoning. The proposed Planned Development would also include above ground fueling tanks and outside display. The proposed zoning change would exclude any residential component from the zoning district.

Don Paschal, with Paschal Consulting, agent for the applicant, Ray Huffines, submitted an application for an amendment to the Land Use designation of the Comprehensive Plan and a zoning change; both mentioned above. The Unified Development Code “Use Chart” allows “Automobile Sales and Service” by SUP in the MX-C zoning district. However an “Auto Body Shop” is a permitted use only in C-3 Commercial and Industrial zoned districts.

The rezoning request is to allow for both “Automobile Sales and Service” and “Auto Body Shop” in this proposed project. In order to accommodate both of these uses; a Planned Development (PD) would be the only option – Planned Development MX-C. Basically the planned development zoning would allow the inclusion of both proposed uses and the Land Use amendment. Along with the proposed zoning change, Staff requested that the residential element of the MX-C base district be removed from the uses within the PD development regulations. Removal of the residential element would be necessary because if the zoning change is approved to allow either or both the “Automobile Sales and Service” and “Auto Body Shop” a Comprehensive Plan Amendment would be necessary for this site. However as mentioned above – the Land Use Amendment process would be done first.

- Neither “Automobile Sales and Service” nor “Auto Body Shop” uses are allowed in the *Mixed Use Non-Residential Land Use* designation of the City of Corinth Comprehensive Plan but the MX-C zoning designation allows for some residential.
- “Automobile Sales and Service” and “Auto Body Shop” uses are allowed in the *Commercial Land Use* designation. This is where the removal of the residential element of the proposed ordinance is necessary as the residential element is not a component of the Commercial Land Use.

CONFORMANCE TO THE COMPREHENSIVE PLAN

The Unified Development Code requires consistency between a Zoning Map Amendment (Rezoning) and the Comprehensive Plan. If the Rezoning occurs which would require a Comprehensive Plan Amendment, this property would not be compatible with the surrounding Future Land Uses.

To summarize; the Comprehensive Plan Amendment of the Future Land Use Designation is necessary but wouldn’t make sense without recommending approval of the Rezoning.

FINANCIAL SUMMARY

Source of Funding: No funding is required.

ATTACHMENTS / SUPPORTING DOCUMENTS

Location Map

Future Land Use Map

Associated Land Use Designations

Application Packet:

Comprehensive Plan Statement of Intent

Comprehensive Plan Amendment Exhibit

Submitted By: Barbara Cabbage, Planning and Development Manager
Department: Planning and Development

Finance Review: Yes NA

Legal Review: Yes NA

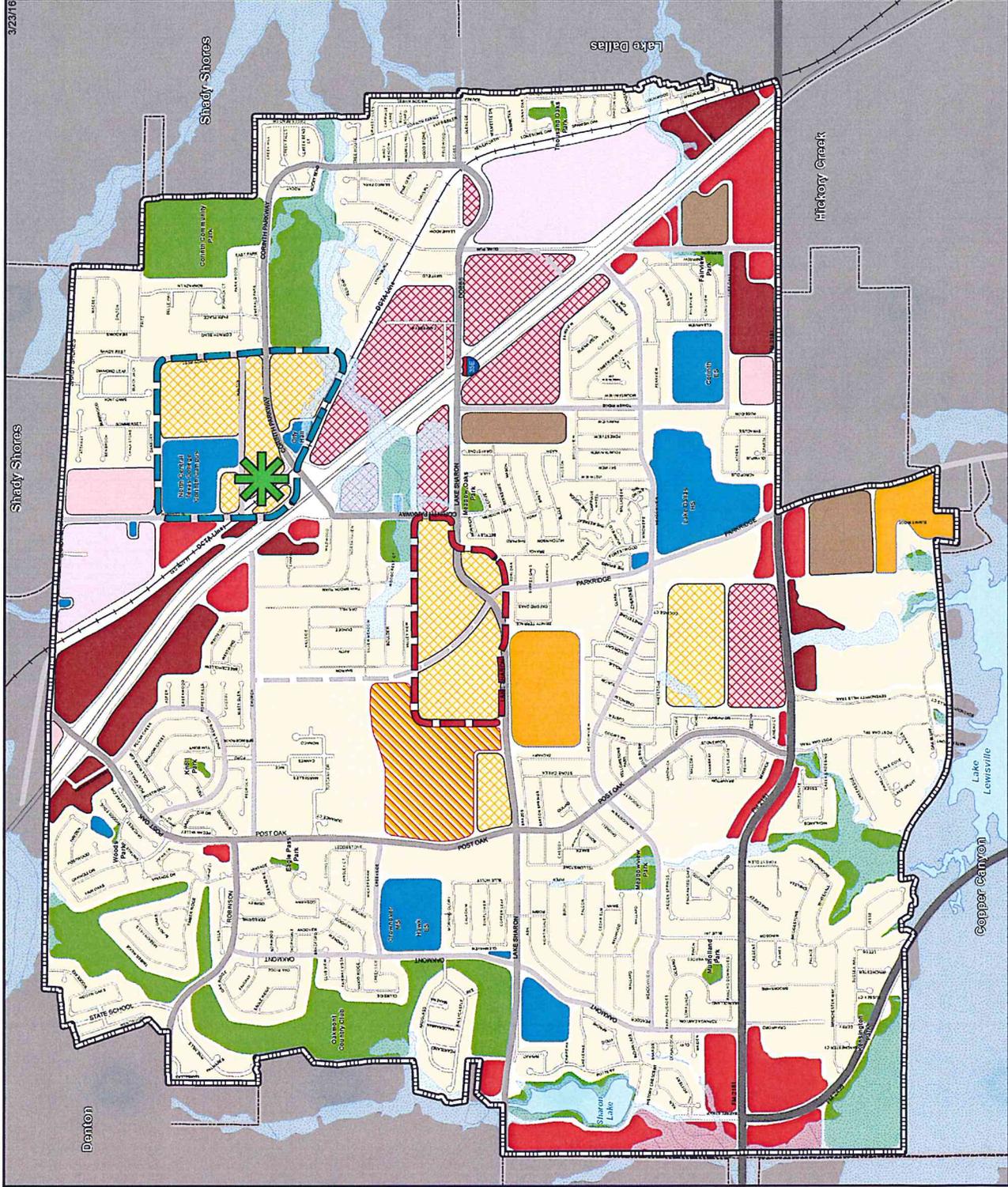
Director Review and Approval:

Future Land Use Plan

- Future Land Use**
- Low Density Residential
 - Medium Density Residential
 - High Density Residential
 - Mixed Residential
 - Mixed Use with Residential
 - Parks and Open Space
 - Public/Semi-Public
 - Mixed Use Non-Residential
 - Office/Business Park
 - Retail
 - Commercial
 - Industrial
 - Multi-Modal Transit Center
 - Transit Oriented Development
 - Corinth City Center
- Road Types**
- Major Arterial
 - Minor Arterial
 - Collector
 - Corinth City Limits
 - FEMA 100 Year Floodplain

Plate 4-1

Note:
A Comprehensive Plan shall not constitute zoning district regulations or establish zoning district boundaries.



3/23/16

Shady Shores

Denton

Shady Shores

Lake Dallas

Hickory Creek

Copper Canyon

Lake Lewisville

COMPREHENSIVE PLAN

CURRENT LAND USE DESIGNATION

This category is specifically designed to address land use along Interstate 35-E, a concern expressed by the members of the Steering Committee. Mixed use non-residential is intended to incorporate planned business campus developments, office, retail, and light assembly/ warehouse distribution centers into one general area with an emphasized focus on quality design and appearance. Public spaces, design regulations, and landscape standards should be strictly enforced in order to ensure a positive visual perception along Interstate 35-E. Although this classification is intended to promote a mixture of non-residential uses, higher density residential options may be considered but should not account for more than 10 percent of the total land use area. It is recommended that mixed use non-residential account for 6.3 percent of the future land use within Corinth.



Mixed Use Non-Residential

PROPOSED

Commercial land use encompasses a wide range of services and uses including banks, automobile-related services, manufactured home sales, self storage units, and lodging locations. Commercial uses often locate along major thoroughfares and freeways in order to ensure adequate accessibility. The challenge lies in the fact that commercial uses generally have a greater need for outside storage areas and such areas tend to lessen the visual perception of such roadways. Commercial land uses in Corinth compose 3.4 percent of the total future land use and are primarily located in areas with existing commercial functions



Commercial

PASCHAL CONSULTING

DONALD E. PASCHAL, JR.

904 Parkwood Court McKinney, TX 75070

Tel & Fax 972-529-1325

email: don@paschalconsulting.com

April 5, 2016

Mr. Fred Gibbs
Director of Planning & Community Development
3300 Corinth Parkway
Corinth, Texas 76208

Subject: Comprehensive Plan Amendment Letter of Intent – Huffines Auto Dealerships

Dear Mr. Gibbs:

Please accept this letter, on behalf of Mr. Ray Huffines, as an explanation of the proposed Comprehensive Plan Amendment for approximately 13.773 acres of land generally located south of the intersection of Meadows Oak Drive and I-35 E, and east of Tower Ridge Drive. We are requesting a Comprehensive Plan Amendment upon recommendation by the staff of the City of Corinth.

The Zoning Map indicates that the site is currently zoned as Mixed Use – Commercial (MX-C); we are submitting a request for a Planned Development concurrently with this application for a Comprehensive Plan Amendment. According to the Master Plan, the site is designated as Mixed-Use, Non-Residential Uses. We respectfully request to modify the existing land use designation to Commercial Uses plus allowing automobile sales and the related components as specified in the proposed PD ordinance. An exhibit supporting this application has also been submitted.

Huffines Auto Dealerships has operated franchised automobile car dealerships in Corinth for over 20 years and throughout that period of time has been a positive corporate citizen of the City, contributing to the community tax base by property improvement, inventory tax, sales tax, and quality jobs. Perhaps equally important is that Huffines has consistently maintained its property in a quality fashion and has demonstrated the highest standards of service to its customers, whether in the area of auto sales, vehicle service, or parts distribution. The heritage that Huffines has exhibited mirrors the image and reality of conscientious community service that the City of Corinth strives to provide and prides itself on.

The existing Corinth dealership site is inadequate for the existing dealerships to remain on the property. The reality of I-35E ROW acquisition by the TXDOT for highway widening and reconstruction has rendered the property unusable in its present configuration and impractical to re-build. Additionally, both dealerships require new facilities to fit the design standard of appearance, quality, and accessibility required for both current business needs as well as growth dictated by the automobile market. The existing site is simply not usable as a quality car dealership due to the loss of property to TXDOT ROW making the site too small and losing virtually all parking in front of the existing buildings.

The magnitude of this project will have a positive impact on Corinth, including: property and building values that are multiples of the current site; up to two and a half times the inventory which translates to inventory value, and multiple times more service and parts, which is also subject to sales tax. Additionally, employment will be expanded immediately upon opening, and will grow about 30% as the site matures; these will be quality jobs in the workforce and are expected to average \$55,000 per position or more. We will provide more specifics as needed in the review process of the economic impact with our final submittal.

The proposed Comprehensive Plan Amendment will have no adverse effect on public health, safety and welfare for the citizens of Corinth. The plan for this site includes an enhanced landscape buffer that will beautify the I-35 corridor and set an aesthetic standard for future developments. The open design of the project promotes desirable air flows through the property thus benefiting all adjacent properties and the community in general. Night and safety lighting is provided at low level thus eliminating intrusion to adjacent properties. The open / campus design of the project contributes to an uncluttered appearance and certainly prevents any over-crowding of the subject property and adjacent areas. Multiple ingress-egress drives are provided thus disseminating traffic to multiple access points; additionally, a mutual access drive allows positive development for the community for the commercial to the north of the subject site. A unique drainage collection system along with detention facilities will provide for filtering of runoff which provides a positive benefit for all storm water runoff. Preservation and creation of new quality jobs is certainly a benefit that contributes to the economic welfare of the community. Architecturally, the design of the proposed facilities is high quality design and is consistent with the City's Architectural Design Ordinance thus enhancing the built environment. Finally, the extensive front setback and intensive landscape along the frontage in particular (although the entire perimeter is appropriately landscaped) will be the highest quality landscape of any project to date in Corinth. The combination of all the above elements provides quality benefits to the public health, safety, and welfare of the community.

A list of requested waivers and variances has been included in the Planned Development Standards; it is noted that the waivers or variances have been significantly reduced from the prior application. We have endeavored to consider opportunities that will positively impact adjacent properties and we will be pleased to work with these property owners

and the City to help enhance the overall commercial opportunities for the site. A specific list of what we are requesting compared to the UDC requirements has been submitted concurrently with this letter.

Thank you in advance for your consideration of this request. Please do not hesitate to contact me with any questions or comments regarding this application.

Sincerely,



Donald E. Paschal

Project Coordinator and Agent for:

S. Ray Huffines
CEO & President, Huffines Auto Dealerships

Consultants:

Don Paschal Consulting – 972.529.1325
SUP and other approvals Facilitator; Project Coordinator

G & A Consultants and EG Group – 972.436.9712
Civil Engineering, Site Development, and Landscape Architecture
Robert Dollak, PE; Randi Rivera, A.I.C.P.; Ron Stewart, A.S.L.A

APDG, A Plus Design Group – 972-724-4440
Facility Architecture and Site Design
Trent Clark



1.03.03. Comprehensive Plan Adoption or Amendment

A. Creation of a Comprehensive Plan

The [Planning and Zoning Commission](#) shall have the responsibility to recommend to the [City Council](#) the adoption of or the amendment to a [Comprehensive Plan](#) for the physical development of the City, including any areas outside its boundaries that, in the judgment of the [Commission](#), bear relation to the planning of the city.

1. Purposes of Comprehensive Plan

The Comprehensive Plan shall be made with the general purpose of guiding and accomplishing coordinated, adjusted, and harmonious development of the City and its environs which will, in accordance with present and future needs, best promote the health, safety, order, convenience, prosperity, and general welfare, as well as efficiency and economy in the process of development.

2. Studies in Preparing Comprehensive Plan

In the preparation of such Comprehensive Plan, the [Commission](#) shall make careful and comprehensive surveys and studies of present conditions and future growth of the city, with due regard to its relation to neighborhood territory.

3. Contents of Comprehensive Plan

The Comprehensive Plan with the accompanying maps, charts, and descriptive matter shall show the recommendations of the [Commission](#) for the development of the City.

B. Adoption and Amendments of the Comprehensive Plan

1. Commission Recommendation for Adoption and Amendments Required

Before the [City Council](#) takes action on the adoption or amendment of a Comprehensive Plan, a recommendation pertaining to such [Council](#) action shall be made by the [Planning and Zoning Commission](#).

2. City Council Action on Adoption and Amendments

After the prerequisite of [Section 1.03.03. B.1](#) has been fulfilled, the [City Council](#) may adopt or amend a Comprehensive Plan by ordinance.

3. Public Hearings

- a. Before the [Planning and Zoning Commission](#) recommends action to the [City Council](#), the Commission shall hold at least one public hearing.
- b. Before the [City Council](#) considers action on a Comprehensive Plan adoption or amendment, the Council shall hold at least one public hearing.



C. Consistency between a Zoning Map Amendment (Rezoning) and the Comprehensive Plan

1. Consistency Rule

A Zoning Map Amendment (Rezoning) shall be consistent with the Comprehensive Plan.

2. Cases Requiring a Comprehensive Plan Amendment Prior to Rezoning Approval

- a. There are cases where the Council may wish to approve a rezoning request that is not consistent with the Comprehensive Plan.
- b. If this case occurs, the Council may amend the Comprehensive Plan and related maps prior to the approval of the rezoning request.
- c. This Comprehensive Plan amendment may occur jointly with the rezoning request, provided the Council acts first upon the Comprehensive Plan amendment.

PUBLIC HEARING / BUSINESS ITEM # 12 & 12a

Planning and Zoning Commission Special Session

April 18, 2016

AGENDA ITEM

- 12. PUBLIC HEARING:** TO HEAR PUBLIC OPINION REGARDING A REQUEST FROM THE APPLICANT RAY HUFFINES AUTHORIZED REPRESENTATIVE FOR DWIGHT WALKER, MANAGING GENERAL PARTNER FOR THE PROPERTY OWNERS DOROTHY WILLIAMS FAMILY PS LTD AND R. O. DUNCAN FOR A ZONING CHANGE FROM MX-C, MIXED USE COMMERCIAL TO PLANNED DEVELOPMENT MX-C ON 13.773 ACRES OF LAND SITUATED IN THE E. A. GARRISON SURVEY, COUNTY ABSTRACT 511 BEING TRACTS 36, 35, 34, 33, AND 24 IN THE CITY OF CORINTH, DENTON COUNTY, TEXAS.
- 12a. BUSINESS:** Consider and act on a zoning change from MX-C, Commercial to Planned Development MX-C on 13.773 acres of land situated in the E. A. Garrison Survey, County Abstract 511 being tracts 36, 35, 34, 33, and 24 in the City of Corinth, Denton County, Texas.

APPROVAL PROCESS

Public hearings will be held and a recommendation will be required to be presented to City Council. This **Zoning Change request** as well as the applicant's requested Comprehensive Plan Land Use Amendment for this site are intertwined and it would be virtually impossible and impractical to discuss one without consideration of the other. However, when it is time for the Commission to make a recommendations, the Comprehensive Plan Amendment must be acted on first per the UDC requirements. Because as mentioned below, a Zoning Map Amendment (Rezoning) must be consistent with the Comprehensive Plan.

The recommendation of the Planning and Zoning Commission will be forwarded to the City Council for final action at the May 5, 2016 City Council regular session meeting.

STAFF RECOMMENDATION

Staff recommends denial of the proposed rezoning request due to a lack of conformance with the City of Corinth's Comprehensive Plan and that the proposed use in whole is not a good fit in the area selected. This is not only Staff's professional opinion; but the vision of the Steering Committee for Corinth's Comprehensive Plan that was approved by City Council in 2010.

However, should the rezoning request be approved, Staff suggests the following be required:

- Residential adjacency standards be adhered to.
- Provide foundation plantings.
- Remove the "heavily treed lot provision" out of the Development Standards.
- Specifically call out and delineate the proposed display where vehicles will be parked for viewing at or near the sales buildings.
- Follow the lighting requirements.
- Provide accurate calculations on the elevation sheets.
- The area proposed is not an appropriate location for a Body Shop – if the rezoning is recommended the body shop should be relocated away from Tower Ridge Road.

- No display of vehicles in the required landscape easement along the frontage.
- Provide the proposed “form of solid or opaque fencing” to be used around dismantled or wrecked cars.
- Include the location of easements.
- With the PD Design Statement include a list of all applicable special development regulations or modified regulations to the base zoning district; plus a list of the requested Subdivision Waivers or other applicable development regulations.

AGENDA ITEM DESCRIPTION

The Applicant is requesting a zoning change from MX-C, Mixed Use Commercial to Planned Development MX-C. Phasing of the project has been established in the Design Statement included in the agenda packet as well as a provided exhibit in the Site Plan packet. The proposed planned development master plan includes development of land and construction of new (KIA and Subaru) automobile dealerships, with service areas, a used car facility, and an auto body shop. This proposed development would cover 13.773 acres of land with the new and used dealerships and service fronting on IH 35E and the auto body shop would be located closer to and fronting Tower Ridge Road. There is one driveway entrance from Tower Ridge Road and three driveways are proposed on IH 35E, however, TXDoT approval and permitting will be required. With this zoning change request, if approved, an amendment to the Comprehensive Plan Future Land Use Element would need to be acted on first.

With regards to the proposed zoning regulations with the Planned Development MX-C District there are elements of the UDC standards of the base district that are being proposed by the Applicant that do not conform to the UDC minimum standards (i.e. lighting requirements, and exterior façade requirements, roof design, and screening between residential uses and non-residential uses and zoning.) The “Auto Body Shop” use is not allowed in MX-C. It is allowed in C-3, Commercial and in I, Industrial zoning districts.

NOTIFICATION TO PUBLIC

Prior to the Planning and Zoning Commission meeting; there are seven property owners whose properties fall within 200’ of the subject properties. Public hearing signs were placed on Tower Ridge Road and IH 35E. No letters of response have been received.

SURROUNDING PROPERTIES ZONING

- | | |
|--------------------|--|
| • Subject Property | MX-C, Mixed Use Commercial |
| • North | MX-C, Mixed Use Commercial |
| • South | MX-C, Mixed Use Commercial |
| • East | MX-C, Mixed Use Commercial with IH 35E divider |
| • West | MF-3, Multi-Family Residential |

SURROUNDING PROPERTIES EXISTING LAND USE

- | | |
|--------------------|--|
| • Subject Property | Undeveloped |
| • North | Residential |
| • South | Commercial – Destiny Dallas |
| • East | Low Density Residential / Undeveloped / Office with IH 35E divider |
| • West | Low Density Residential / High Density Residential |

SURROUNDING PROPERTIES FUTURE LAND USE

- Subject Property Mixed Use Non-Residential
- North Mixed Use Non-Residential
- South Mixed Use Non-Residential
- East Mixed Use Non-Residential with IH 35E divider
- West High Density Residential

PROPOSED ZONING

The Applicant is requesting a zoning change from MX-C, Mixed Use Commercial to Planned Development MX-C that would allow for an “Automobile Sales and Service” and “Auto Body Shop”.

EXPLANATION OF PROCESSES OF THIS PROPOSED PROJECT

Don Paschal, with Paschal Consulting, agent for the applicant, Ray Huffines, submitted an application for an amendment to the Land Use designation of the Comprehensive Plan and a zoning change; both mentioned above. The Unified Development Code “Use Chart” allows “Automobile Sales and Service” by SUP in the MX-C zoning district. However an “Auto Body Shop” is a permitted use only in C-3 Commercial and Industrial zoned districts.

The rezoning request is to allow for both “Automobile Sales and Service” and “Auto Body Shop” in this proposed project. In order to accommodate both of these uses; a Planned Development (PD) would be the only option – Planned Development MX-C. Basically the planned development zoning would allow the inclusion of both proposed uses and the Land Use amendment. Along with the proposed zoning change, Staff requested that the residential element of the MX-C base district be removed from the uses within the PD development regulations. Removal of the residential element would be necessary because if the zoning change is approved to allow either or both the “Automobile Sales and Service” and “Auto Body Shop” a Comprehensive Plan Amendment would be necessary for this site. However as mentioned above – the Land Use Amendment process would be done first.

If the Commission decides that the proposed uses are appropriate at this location, a zoning change would be necessary to allow either or both “Automobile Sales and Service” and “Auto Body Shop”. The removal of the residential element of the MX-C district proposed is necessary as well due to residential not being a component of the Commercial Land Use designation.

CONFORMANCE TO THE COMPREHENSIVE PLAN

- Neither “Automobile Sales and Service” nor “Auto Body Shop” uses are allowed in the *Mixed Use Non-Residential Land Use* designation of the City of Corinth Comprehensive Plan but the MX-C zoning designation allows for some residential.
- “Automobile Sales and Service” and “Auto Body Shop” uses are allowed in the *Commercial Land Use* designation. This is where the removal of the residential element of the proposed ordinance is necessary as the residential element is not a component of the Commercial Land Use.

To summarize; if the Commission decides to recommend approval of the zoning change, an amendment to the Comprehensive Plan is necessary first.

POTENTIAL FUTURE PROCESSES

If the request for zoning change is approved *and* the future land use designation for the 13.773 acres is approved, Staff would recommend that the City consider amending the land use designation of the adjoining properties as well.

FINANCIAL SUMMARY

Source of Funding: No funding is required.

ATTACHMENTS / SUPPORTING DOCUMENTS

Zoning Map

Use Chart

Application Packet:

PD Letter of Intent

Applicant's Economic Impact Summary

Design Statement and Concept Design Map with Proposed PD Standards and Regulations

List / chart of anticipated waivers / variances / alternative compliance to UDC requirements

Detailed Site Plan Documents including:

Site Plan

Landscape Plan

Elevations

Photometric Plan

Signage and Fence Renderings

Monument Signage Calculations

Fencing and Signage Exhibit

Phasing Plan

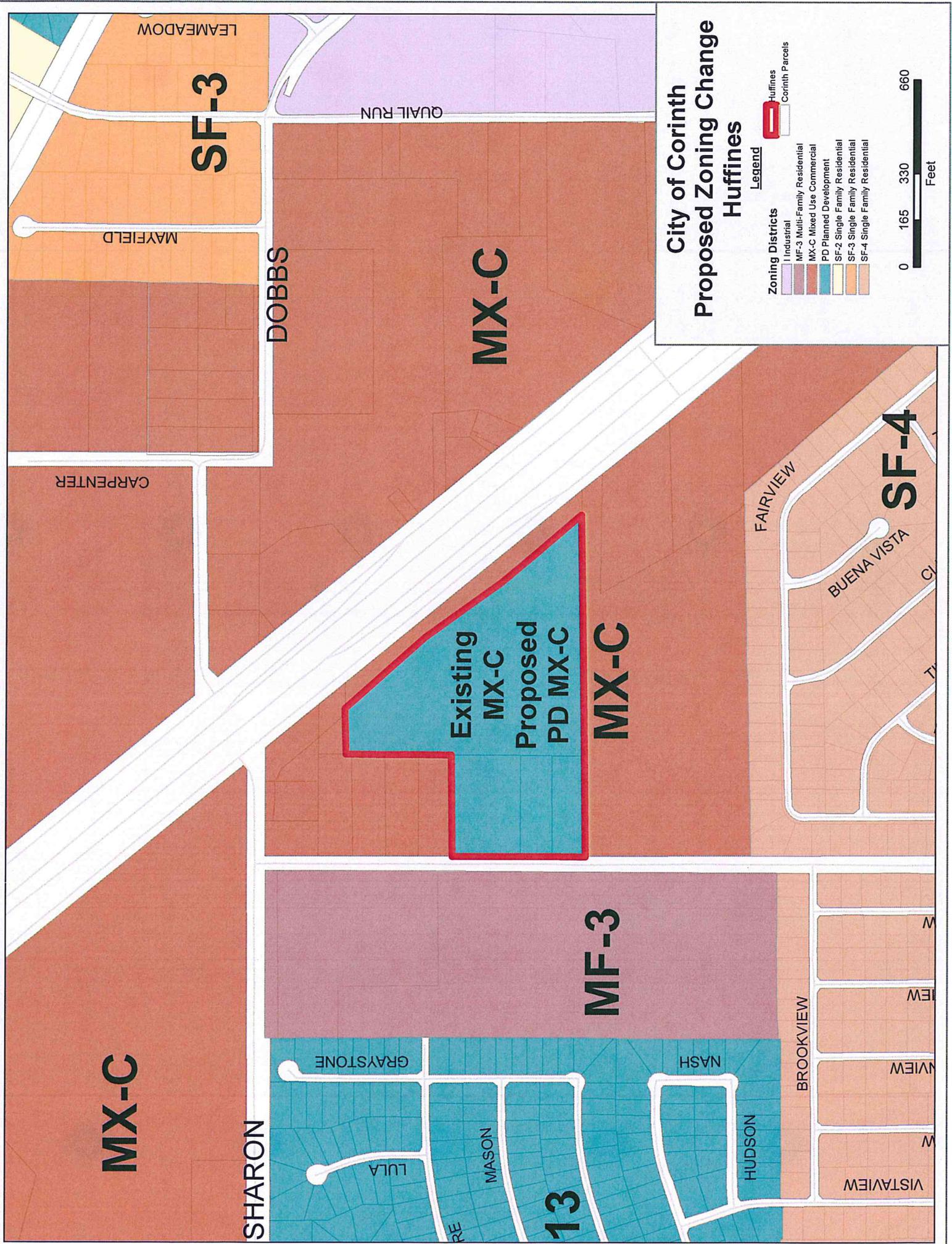
Submitted By: Barbara Cabbage, Planning and Development Manager

Department: Planning and Development

Finance Review: Yes NA

Legal Review: Yes NA

Director Review and Approval:



City of Corinth Proposed Zoning Change Huffines

Legend

| |
|--|
| █ Huffines |
| █ Corinth Parcels |

Zoning Districts

- █ Industrial
- █ MF-3 Multi-Family Residential
- █ MX-C Mixed Use Commercial
- █ PD Planned Development
- █ SF-2 Single Family Residential
- █ SF-3 Single Family Residential
- █ SF-4 Single Family Residential

0 165 330 660
Feet

PASCHAL CONSULTING

DONALD E. PASCHAL, JR.

904 Parkwood Court McKinney, TX 75070

Tel & Fax 972-529-1325

email: don@paschalconsulting.com

March 23, 2016

Mr. Fred Gibbs
Director of Planning & Community Development
3300 Corinth Parkway
Corinth, Texas 76208

Subject: PD Ordinance / Zoning Request Letter of Intent – Huffines Auto Dealerships

Dear Mr. Gibbs:

This Letter of Intent is submitted on behalf of Mr. Ray Huffines. I am designated as the agent for the subject application.

As you know, we originally submitted our initial Letter of Intent and SUP application along with supporting materials for a new Huffines Auto Dealerships facility at the property identified below on May 15, 2015. Over the ensuing several weeks / months, we worked with City Planning Staff to modify the requested zoning amendment from a SUP to a Planned Development Zoning as suggested by City Staff. The project unfolded and we worked through initial meetings with the Planning Staff resulting in revision and amendment of the application to a Planned Development which also required a modification of the Comprehensive Plan.

From May, 2015 through September, 2015 we continued efforts to refine the PD standards so as to conform to the design objectives of the City and to be more consistent with the UDC. Several exceptions or variances to certain UDC provisions which are at odds and conflict with other City objectives were crafted and we worked to coordinate design issues with the franchise dealership manufacturers.

Since the denial of our original request on October 15, 2015, we have reassessed our positions and sought out input to better understand issues of concern to both City Staff and the City Council.

Reassessment of the Project:

We began the reassessment process by meeting with both the Mayor and Mayor Pro Tem to secure their input and thoughts relative to considerations and actions we could take to demonstrate to the City and the Council that we had truly completed a due

diligence exercise that addressed all the issues and concerns. The Mayor and Mayor Pro Tem suggested that we:

1. Meet with key city staff to secure their input –
 - A. This led to two meetings / discussions – one with Jason Alexander and the other with Fred Gibbs. From these meetings we received input on alternate locations and ideas that might be positively received by the City.
 - B. The principal issue was full research on the potential for locating the dealership on the east side of I-35 generally opposite the current Huffines location.
2. Meet with each City Councilmember to secure individual input of their opinions and views.
 - A. We were careful to only seek input and advice on the Councilmember opinions and adjustments / changes that might open the door of reconsideration.
 - B. We were also careful to never ask for a specific vote or action based on my strong belief in objectivity in the public hearing process.
 - C. The general objective was to generate a dialogue that was open and intended to guide us as applicant if we chose to resubmit to another review process and to include information or development understanding that could address prior concerns.
 - D. While we always felt that our proposal was quality and incorporated positive economic development benefits for the City, we also believe that from the comments received:
 - i. we could have enhanced the perception and reality of blending with the City Comprehensive Plan better,
 - ii. we could have illustrated a better understanding of how the project could achieve more city objectives, and
 - iii. we could have better illustrated that there were simply no other practical options to the proposed location if we were to maintain the Huffines Automotive presence in Corinth

We spent almost three months working through the above issues; virtually every effort was taken to fulfill the requested due diligence actions suggested by the Mayor and Mayor Pro Tem as well as the suggestions of all the staff and other Council Members. We met with all the requested parties and had open and fruitful dialogue. While it is impossible to know the extent to which we have opened thoughts or perceptions, we are grateful for the openness and candid discussions with each person. We do believe that there is better understanding of our project, why we are re-proposing it, that we have adjusted several components of the request in a manner suggested, and that the overall project better meets the City development objectives than our previous submittal. It will be our hope that the City Staff, the P & Z, and the City Council will concur with our amended request.

Specific actions in our due diligence included:

- Re-evaluation of the existing location to find ways to keep the dealerships at the current, but expanded location. Efforts included attempting to identify additional

property connecting to the current site. It soon become fully apparent that we would not be able to secure any additional property to enlarge the site. We also did everything short of structured parking to find a way to use the site in its current configuration; unfortunately we simply cannot add enough inventory parking to make the site viable.

- Detailed consideration of property along the east side of I-35 opposite the current location. We identified all properties and were not able to assemble a sufficient size parcel to make the location viable. If requested, I would be pleased to provide details on the properties and reasons this consideration was not possible.
- Discussed issues of the City Comp Plan and how we can better blend with those objectives.
- We determined that dropping an approximately one acre lot from our application for rezoning and site plan approval could enhance the potential for both our site and the adjacent property to the North of our site to better meet City objectives.
- We redesigned the mutual access easement on our north boundary to better serve all the properties, including the commercial property up to Meadows Oak.
- We reassessed the variances / exceptions to our PD Ordinance and determined that with our minor redesign we could eliminate several exceptions.
- Other variances / exceptions were reevaluated and determined to be unnecessary; some variances / exceptions were retained and are detailed in the amended PD regulation document.
- While we do not want to relocate out of Corinth, our full due diligence efforts did look at optional sites outside of Corinth that could be pursued if our zoning efforts in Corinth are not productive.

Resubmitted PD Ordinance Purpose and Property Location; Existing and Proposed Zoning:

Huffines Auto Dealerships is requesting approval of a PD Ordinance for 13.773 acres (about an acre less than the prior 14.746 acre request); it is the same location along the west frontage road of I-35 E, approximately 350 to 400 feet south of Meadows Oak, partially adjacent to and east of Tower Ridge, and adjacent to the existing RV / mobile-home park to the south. The existing zoning is MX-C. We are proposing a change of zoning to a PD with a base zoning of MX-C with additional detailed uses and regulations which will allow for Automobile Dealership / Sales and full service for two separate new automobile franchises along with a separate Previously Owned Sales facility.

Background:

As previously discussed, Huffines Auto Dealerships has operated franchised automobile car dealerships in Corinth for over 20 years and throughout that period of time has been a positive corporate citizen of the City, contributing to the community tax base by property improvement, inventory tax, sales tax, and quality jobs. Perhaps equally important is that Huffines has consistently maintained its property in a quality fashion and has demonstrated high standards of service to its customers, whether in the area of

auto sales, vehicle service, or parts distribution. The heritage that Huffines has exhibited mirrors the image and reality of conscientious community service that the City of Corinth strives to provide and prides itself on.

As stated earlier in this letter of transmittal, the existing Corinth dealership site is inadequate for the existing dealerships to remain on the property. TXDOT right of way acquisition for highway widening and reconstruction has rendered the property unusable in its present configuration and impractical to re-build. Additionally, both of our Corinth franchised dealerships require new facilities to fit the design standard of appearance, quality, and accessibility for current business needs as well as growth dictated by the automobile market. The existing site is simply not usable for quality car dealerships due to the loss of property to TXDOT ROW making the site too small and losing virtually all parking in front of the existing buildings. I am pleased to report that all those we met with voiced the thought that they did not want to lose us as a corporate citizen.

Rationale for site selection:

Being very aware of the planning and development objectives of the City of Corinth, we have now spent about ten to twelve months of intensive study of alternative sites and assessed both immediate and longer term impacts on Community objectives. The proposed site was chosen for multiple reasons including:

- It is not in the City's designated prime area for corporate office and retail development.
- It is large enough to provide both immediate dealership use and subsequent expansion which will be required for dealership growth.
- It maintains a high profile office and retail / restaurant site at the southwest intersection of I-35E and Meadows Oak.
- It provides a transitional use north of the RV and mobile home park and other business uses while preserving some of the treed areas as buffers.
- It thus preserves other properties' potential for corporate and retail / restaurant uses while contributing to the transition between other uses and our proposed use in a manner that will be complimentary to community objectives.

Proposed Facilities:

It is the intent of Huffines Auto Dealerships to build completely new facilities on the proposed site, complete with fully compliant infrastructure (utilities, pavement, and drainage), landscaping and buffering, site amenities, and access. The site will serve as a model for new era dealership development. Facilities proposed for this resubmittal include:

- A new Kia dealership sales facility planned for an 9,100 SF building
- A new Subaru dealership sales facility planned for a 11,000 SF building (which will include a parts area)

Note: both of the above two franchise dealerships are currently operating in

Corinth area and are highly sought after vehicle franchises due to quality, reliability, and user demand

- A pre-owned venue / sales office planned for a 6,000 SF building
- An initial dual franchise service / maintenance facility planned as a 38,500+ SF building that will subsequently be expanded as growth dictates (additional 4,000 SF).
- Provision for a second service building area behind to the south of and parallel to the initial service building that will add 19,000+ SF when needed.
- Provision for a future specialty and body repair facility projected at 21,000 SF
- Parking for over 50 customer vehicles.
- Parking for over 60 employee vehicles at opening; expansion to provide additional employee parking spaces with expansions and new facilities for an expected 80 to 85 employees.
- Initial parking for 525+ display and display & inventory vehicles and future parking expansion to provide about 50% more display & inventory vehicles
- The resubmittal does delete approximately 1 acre of inventory parking to allow the property to be blended with other commercial, retail, and / or office development.

Only customer sales facilities will be oriented for full building direct visibility from I-35E with service - maintenance facilities located behind the three sales buildings which are thus are buffered by substantial sales facilities having full visibility from I-35.

Design and Architectural Considerations:

Buildings - We have successfully completed concept plans for building architecture, specifically exterior elevations that comply with City design and material standards. Masonry (brick, stone, tilt wall) and glass comprising 90%+ of all the proposed building facades. We have and will continue to work with staff in discussing these design considerations to assure compliance with City objectives while meeting the design requirements of the manufacturers, which is an equally important consideration so as to maintain both highly prized franchises. Additionally, we have worked to blend the architecture of all buildings to fit harmoniously on the property. A complete description of the design concept is included as required in the PD Ordinance Draft provided with this submittal.

Landscaping – Huffines proposes to follow and go beyond the City standards by incorporating innovative landscape components along the I-35E frontage areas. The overall landscape design will be environmentally friendly including landscape bed areas with drip irrigation projected for all beds. Existing tree masses will be preserved where possible and will be used to serve as a perimeter buffer thus preserving a positive component of natural vegetation on the property. Additionally, we are pleased to have developed a linear bio-swale / detention area within the front landscape setback that is extremely attractive, environmentally friendly, and offers the potential of an overall North to South community landscape and drainage design that would be a strong identity and

community asset for Corinth. A complete description of innovative design concepts is included in the revised PD Ordinance Draft based on Staff comments.

Security – a low pipe rail will encircle the property with low shrubbery / vegetation concealing much of it along the frontage and visible areas of the two east-west property lines near I-35E. The pipe rail will be supplemented by other security fencing around the balance of the property that will be flexible enough to avoid damage to preserved native trees. Details and UDC exception for the screening are included in the PD draft.

Signage – separate monument signage as required for each sales building / franchise will be incorporated along the frontage at required setbacks. The distance from I-35 necessitates that we propose exceptions from the UDC provisions for the monument signage. It had been our understanding that the City was considering modification of the sign ordinance, which has just recently been approved. Previously, we had suggested that some of the proposed sign provisions we were requesting might serve as a test case for modification of the existing monument signage requirements. We hope our input was of some value.

After reviewing the new sign ordinance, we are re-submitting the prior request with the expectation that some of the items we are requesting are now can be approved as part of the new ordinance, although we must admit that the newness of the ordinance prevents us fully understanding it nuisances.

We are aware of the City desire to have no pole signs and have attempted to provide reasonable monument signage for a major site on the interstate highway, particularly considering the unusually expansive open space between the property line and the future access road (60 to 70 feet). Thus, modification of the existing (at time of this submittal) standards is a critically needed issue. It is our opinion that the prior adjustments we requested may have been addressed by the new ordinance provision of “performance based standards” that allow additional size. We believe our monument signs to be within the ordinance except for our request for a “0” setback due the extraordinary distance from the highway.

Additional signage identifying each franchised dealership and the pre-owned sales buildings will be incorporated into structures by providing appropriate locations on the front and sides of the buildings for signage. Again, we believe it can be interpreted that the distance back from the highway of our buildings that the new letter size provisions accommodate our needs. We still have concerns over the placement of signage in the middle 50% of the buildings and with signage on only one side of our structures; thus, we are requesting the necessary ordinance exceptions. It is noted that with the franchise logos on the north and south faces of the structures eliminates our need for pole signs, which are not being requested.

Other – other design elements will be incorporated into the site and discussed with City Staff in the review process. Additionally, other exceptions to the UDC are critical and included in the PD Ordinance draft. A typical example is a concern over residential adjacency issues which arises because there is one vacant, depilated house awaiting

demolition for commercial redevelopment and one non-conforming rent house that will be redeveloped for a commercial / business development and use consistent with the property zoning. The existence of these two houses, even though not zoned for single family residential, and expected to be redeveloped, causes the potential for the residential adjacency ordinance which will negatively impact multiple aspects of the proposed project. To allow isolated and non-conforming uses to negatively impact design integrity and consistency within our campus setting while potentially imposing irrational screening requirements that do not contribute to quality and community value is bad for Corinth and thus, the appropriate exceptions to the conflicting ordinances is requested. However, by eliminating the one acre from our site plan and application, eliminates some of the variances. Additionally, other previously requested variances / exception have be eliminated. All anticipated exceptions to the UDC provisions are addressed in the PD draft regulations; we will be pleased to meet and discuss each of these requests with City Staff and through the review process.

Adjacent site considerations – in conceptual design, we have endeavored to consider opportunities to positively impact adjacent properties. While we have already discussed blending and cooperation with adjacent property owners, we will be pleased to continue to work with those property owners and the City to help enhance the overall commercial opportunities for the community. Specifically, we have modified the north mutual access easement to better contribute to future commercial development and the adjacent owner has expressed that he is in agreement with the new design. As we work through the project to the platting stage we will pursue the required agreements with the adjacent property owner a for mutual access agreement.

Economic Impact:

The magnitude of this project will have a positive impact on Corinth including:

- Property and building values are several times the value of the current site
- Up to 3 times the inventory which translates to inventory value
- Considerable more service and parts, which is all subject to sales tax.
- Employment will be expanded immediately upon opening (from current 55 employees to 70 at opening) and will be expanded at the dealerships mature to an anticipated 85+ over the next decade. It is also noted that these will be quality jobs in the workforce and are expected to average \$55,000 per position or more by opening of phase I of the project.

A statement of expected economic impact has been provided to staff and will be available as well for P & Z, and City Council information. The statement has been modified by the impact of eliminating one acre from our zoning application, but due to construction inflation, there is very little change. There is no projected impact on business volume of auto sales or maintenance due to the one acre reduction. However, when that one acre site is developed as a business use instead of storage of vehicle inventory, the City will actually realize a higher economic impact.

It is understood that following the PD Ordinance process as proposed, we will need to process a final site plan and then a final Plat for the property once the ordinance has been adopted. Below is a list of our consultants and their respective areas of responsibility; City staff is welcome to contact each as needed for relevant issues. Huffines Auto Dealerships is anxious to proceed through the review and approval process and hopes to begin physical work on the property before the end of 2016, as detailed in the site data provided. We will be pleased to work through discussions with the City as needed.

Sincerely,



Donald E. Paschal
Project Coordinator and Agent for:
S. Ray Huffines
CEO & President, Huffines Auto Dealerships

Consultants:

Don Paschal Consulting – 972.529.1325
SUP and other approvals Facilitator; Project Coordinator

G & A Consultants and EG Group – 972.436.9712
Civil Engineering, Site Development, and Landscape Architecture
Robert Dollak, PE; Randi Rivera, A.I.C.P.; Ron Stewart, A.S.L.A

APDG, A Plus Design Group – 972-724-4440
Facility Architecture and Site Design
Trent Clark

Attachment: Estimated Economic Impact



2.06.03. PD, Planned Development

The purpose of this district is to encourage quality and better development in the City by allowing flexibility in the planning and development of projects. A [PD, Planned Development](#) District may be used to permit new or innovative concepts in land utilization or diversification that could not be achieved under conventional zoning approaches. Any combination of residential, commercial, light industrial, public or recreational uses as approved by the City Council may be permitted.

A. General Description and Purpose

1. The PD designation shall be used for the following purpose(s):
 - a. Master planning;
 - b. To carry out specific goals of the [Comprehensive Plan](#), City or public/private partnered special projects;
 - c. Development of mixed use, transit-oriented, or traditional neighborhoods with a variety of uses and housing types; and/or
 - d. To preserve natural features, open space, and other topographical features of the land.
2. The PD designation shall not be used for the following purpose(s):

To obtain [Zoning Variances](#) and [Subdivision Waivers](#) from existing development standards; to secure agreements between an [Applicant](#) and nearby property owners to receive zoning approval; and/or to assign responsibility to the City of private deed restrictions or covenants.

B. Land Area Requirement

1. A PD district requires a minimum of two (2) contiguous acres, except as allowed 2.06.03. B.2 below.
2. A PD district acreage may be less than two (2) acres when carrying out the recommendations of the [Comprehensive Plan](#).

C. Base Zoning District

1. A PD shall contain at least one base zoning district to regulate all uses and development regulations not identified as being modified.
2. If the standards of the base zoning district are amended, then the most recently amended standards shall apply to a PD district unless the standards have been individually listed within the PD adoption ordinance as being different from the base zoning district.
3. Base zoning district amendments are applicable to PD districts.

D. Permitted Uses and Use Regulations

1. See the [Use Chart](#) and all applicable regulations within Subsection 2.07.
2. See Section [2.10.09. PD, Planned Development Application and Review](#).

E. Dimensional Regulations

1. See [Subsection 2.08 Zoning Dimensional Regulations](#).
2. See Section [2.10.09. PD, Planned Development Application and Review](#).

F. Development Standards

1. See the [Zoning Development Regulations](#) in Subsection 2.09 for all applicable regulations.
2. See Section [2.10.09. PD, Planned Development Application and Review](#).

EXHIBIT “B”
HUFFINES KIA SUBARU DEALERSHIP PD MASTER PLAN
CONSISTING OF
PD DESIGN STATEMENT

- i. This project / ordinance shall be referenced as the Huffines Automotive Planned Development Ordinance.
- ii. The owner / developer of the project will be Huffines Automotive Dealerships; the President of the group is S. Ray Huffines.
- iii. The property is located approximately 300 feet south of Meadows Oak, west of the I-35 ROW, north of the Destiny Dallas mobile home park, and partially abuts Tower Ridge Drive. Adjoining Land uses in relationship to the site are:
 - o North – approximately 3 acres zoned Mixed –Use Non-Residential Zoning – MX-C; there is currently a non-conforming single family house on the property, however, the property is being marketed for the commercial uses consistent with the existing zoning.
 - o East – I-35 highway and expanded right of way.
 - o South – approximately 10 plus acres zoned Mixed–Use Non-Residential Zoning – MX-C; approximately 2 to 3 acres of the frontage along I-35 is vacant and appropriate for use as zoned; the balance of the property is used as a non-conforming mobile home park.
 - o West of Tower Ridge Drive near the south half of the proposed project is the clubhouse and leasing office for a multi-family apartment complex; to the north of the multi-family project up to Meadows Oak the property is zoned Mixed–Use Non-Residential Zoning – MX-C and is currently anticipated for a new church site.
 - o East of Tower Ridge Drive and adjacent to the north half of the proposed project are 3 lots of approximately 1 acre each making a total of approximately 3 acres of land zoned Mixed–Use Non-Residential Zoning – MX-C; one of the lots (the one immediately north the subject tract) is currently a non-conforming rent house which the owner plans to remove and transition the property to a commercial use.

The subject property and the properties to both the north and south are all zoned and projected for various commercial uses although the majority of the property to the south is currently and will remain for the foreseeable future a mobile home park. The property to the southwest of the subject property is a multifamily complex and the property generally west to northwest is proposed as a church use. There is currently no connectivity between any of the properties with Tower Ridge serving as a common N-S access collector immediately west of the subject property. The I-35 service roads serve as the prime access along the east frontage of the subject property and the properties to the north and south. Special efforts have been taken to assure that the adjacent

properties (and resultant City development opportunities) are optimized by considering the extension of drainage, utilities, and accessibility which is a special issue considering the limited access nature of the I-35 corridor which restricts ingress and egress.

Thus, joint access is suggested as shown on the proposed concept plan. The joint access concept has been restructured with this re-submittal. The only proposed connectivity between any of the properties is the proposed mutual access easement between the subject property and the property to the north which is designed to open better commercial opportunities for the properties gaining enhanced access from the mutual access easement. When Meadows Oak is extended under and east of I-35 it will become a thoroughfare connecting much of the property west of I-35 to the east side of I-35.

As noted above, there are two adjacent parcels that are not part of the pending site acquisition plus one parcel that is to be purchased, but not zoned; these three parcels are approximately 1 acre each. The land use for all three of these parcels is for business purposes. From south to north: the south most parcel is currently being used as a residential rental property, but is expected to re-develop for business purposes by the owner or others; the next parcel to the north is for sale at this time for commercial uses; the third and north most of the three 1 acre lots is being acquired by the applicant, but is not part of the zoning application. The applicant intends to sell this lot for commercial purposes as zoned; it is anticipated that the lot will be marketed along with the Meadows Oak frontage property thus expanding the potential Meadows Oak commercial use concepts. The applicant will provide the City with a conceptual drawing of how the properties can be blended. The illustrated, the conceptual land use plan for the adjacent properties can easily incorporate the two additional properties although they are situated for independent use and development; it is important, however, to see the conceptual land plan for these adjacent properties from the potential for site visibility from I-35 which enhances potential.

It is noted that the property adjacent to Meadows Oak and one additional property east of Tower Ridge have existing residential use although said use is not in conformity to the land use plan. Both property owners have advised us of their intent to redevelop the properties into commercial uses. We believe it appropriate to exclude the residential adjacency requirements of the UDC from this project since the two parcels are expected to re-develop according to the existing business zoning classifications which the City has established and desires to see become reality; therefore, the UDC residential adjacency should not restrict the business use and the cohesive campus design and architecture proposed. However, the applicant has eliminated most of the variance or exceptions previously requested to simplify the application and zoning ordinance.

- iv. The subject property is approximately 13.8 acres and is proposed as the new location for Huffines Automotive Dealerships in Corinth. It should be noted that the size is approximately 1 acre less than the application considered by the City Council in October

2015; the north 1 acre lot fronting on Tower Ridge Dr has been eliminated from the zoning proposal. While the applicant is acquiring the lot, it is anticipated that the lot will be marketed for commercial purposes as currently zoned along with the balance of the property to the north of the subject property.

The design concept for the site and overall development proposed by this PD and associated Concept Plan is summarized as follows:

To establish a new concept Automobile Dealership for Auto Sales and maintenance based on a campus style land plan that:

- a. Focuses three human scale, moderate sized sales buildings on the I-35 frontage with supporting structures generally behind the sales buildings. Both Phase I and expansion Phase II service / maintenance facilities are bridged or connected between the two new vehicle dealerships, and share a common customer vehicle entrance corridor between the two sales buildings. While it appears that there is one large building encompassing the two primary new vehicle dealerships and service facilities, there are actually two main buildings connected with a multi-bay open air covered service entrance or canopy.
- b. Actual service and maintenance facilities extend to the west behind the sales buildings. A service / collision repair facility is also located behind the dealership sales buildings. The customer sales facilities are oriented for full / direct visibility from I-35E; service and maintenance facilities will be located behind the sales buildings and thus are substantially buffered from view by the dealership sales structures. The customer vehicle service entrance corridor has a direct view from I-35, particularly after the customer has entered the access drive between the two dealership buildings, although the view is recessed to the rear of the sales facilities. While somewhat obscured from the highway by the sales buildings, the service area becomes directly visible as the customer enters the drive between the two primary dealership buildings. Thus, the service / maintenance area is both obscured from the highway but easily viewed by the on-site customer seeking maintenance. Provides required compliance with manufacturer facility design requirements while assuring design integrity and compatibility across the site through building shape, vertical and horizontal articulation, base and accent building materials (similar brick, compatible stone, glass, entry features, and other related materials), and a clean and access friendly ingress and egress plan.
- c. Maintains key location mature trees and supplements existing vegetation with new landscaping in compliance with City standards. Where possible, existing tree masses will be preserved and will serve as a perimeter buffer.
- d. Goes beyond basic requirements of site and landscape design by the inclusion of a front bio-swale within the required landscape setback. The bio-swale filters runoff water from the property through an underground filtration system, and is also landscaped using a combination of shrubs, turf grass, and trees while channeling the runoff through bio-swales that act as a series of detention ponds. They blend with the drop in topography in such a way as to appear as a series of natural stone enhanced basins or dry creeks, which achieve City detention standards. This design

feature is intended to complement the City's objective to transform I-35, as it is rebuilt and expanded into an integrated business corridor, the focal point of which will be the transformation of a large floodplain on the west side of I-35 between Corinth Parkway and Meadow Oaks into a scenic, stone bordered retention / detention area. The subject project enhanced bio-swales will become the initial components of a concept that can be replicated all along I-35 to provide a consistent landscape theme incorporating the stone bordered detention systems (and perhaps including the bio-swale concept) so that it appears that the existing topographic relief all generally feeds into the City Core scenic retention / detention area. If the bio-swale concept is continued up and down the I-35 corridor, the City will have encouraged / facilitated one of the only such "town-wide" bio-swale / detention and integrated community landscape themes in the state and perhaps country. By clustering and appropriately locating the required trees, we will be able to replicate the appearance of natural tree groves blended into the attractive topo of the site, all of which is enhanced by the stone bordered retention / bio-swale system. It is noted that the project also provides for additional detention components on the south end of the project frontage and internally toward the north portion of the property thus taking advantage of the drainage basin divide running through the site.

- e. A low pipe rail will encircle the property. Along the frontage, the pipe rail will be supplemented with low shrubbery and vegetation, concealing much of the pipe rail along the frontage and visible areas along the north and south property lines near I-35. Around the balance of the property, the pipe rail will be supplemented by other security fencing (no less than 6 feet high and no more than 8 feet high) on the property line but flexible enough to avoid damage to preserved native trees.
- f. Separate monument signage as required for each sales building or franchise will be incorporated along the frontage behind TXDOT ROW within the project landscape setback. Additional signage identifying each franchised dealership and the pre-owned sales buildings will be incorporated into structures by providing appropriate locations on the front and sides of the buildings for signage. Details of building signage have been provided illustrating location, distribution, and spacing of signage.
- g. It is pointed out that the NW most portion of the property being acquired (approximately 1 acre) by the applicant is not included in the zoning process to accommodate auto sales. The applicant is proposing to leave this NW most approximate 1 acre as it is currently zoned. It is intended that this will allow that property to be a major component of the commercial / office use anticipated for the property immediately south of Meadows Oak thus providing a positive blend with the other objectives of the City.
- h. The Concept Plan provides for on-site growth based on the projected and current market demand for the Auto franchises which will be relocated from another site to the proposed site. Thus, the City will have built-in property and sales tax growth and expansion of quality jobs as the site matures into its full capability. It is noted that the existing dealership location farther north is inadequate for current design and size requirements for Automobile Dealerships due to inadequate acreage and the reality that the TXDOT R.O.W. takings push the service / access roads too close to the

existing structures to maintain customer and display parking in front of the buildings. The applicant has revisited the existing dealership site potential as requested by the City and was unable to acquire any additional properties adjacent to the site; it has been re-verified that the existing site is inadequate to accommodate the required vehicle inventory or building spaces to serve the business requirements. Therefore, it is expected that the existing dealership site approximately two miles to the north will be redeveloped into uses more suitable for the size of the property.

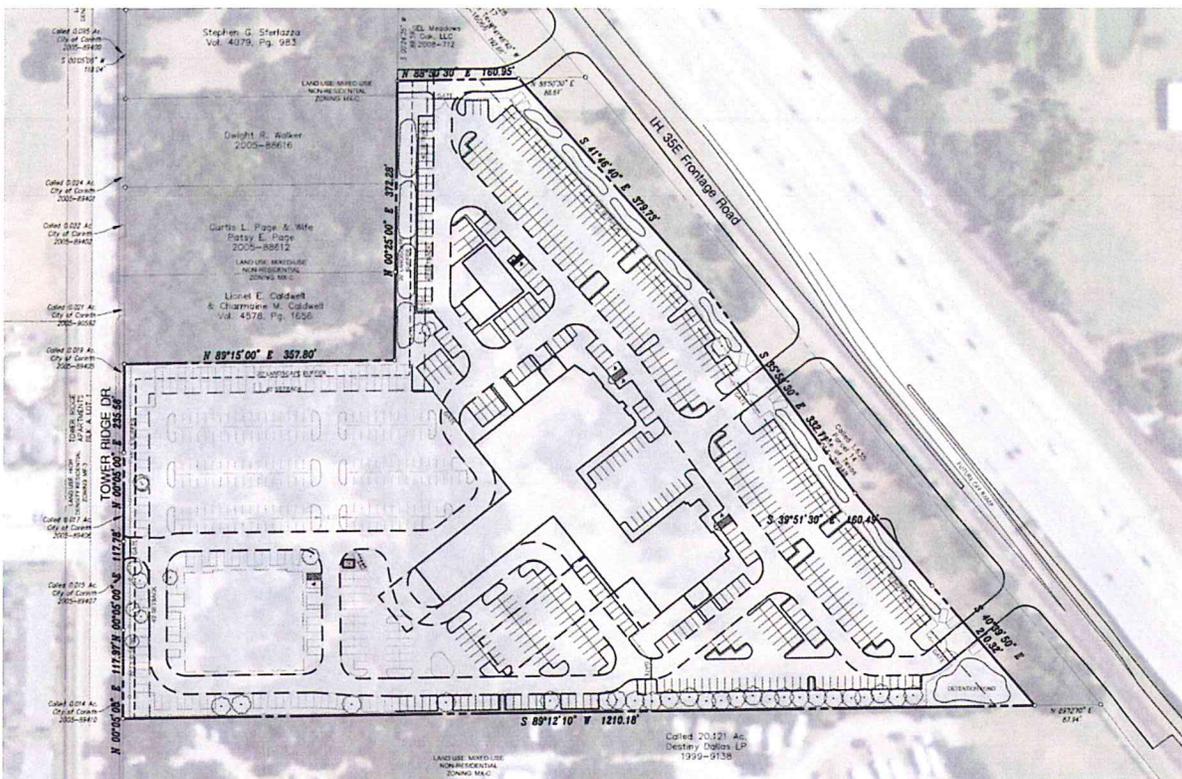
- v. No other existing PD zoning districts are known to exist in the general area near the proposed project.
- vi. The "MX-C" Mixed – Use Commercial District, including the regulations of the Corinth Unified Development Code, per the City of Corinth Unified Development Code has been selected as the base zoning district to regulate all uses and development regulations not identified as being modified by the subject PD regulations. The PD regulations hereinafter provides for the special provision required for the subject use to function as full sales and service automobile dealerships.
- vii. A list of all applicable special development regulations provided by the subject PD or anticipated as waivers or variances to the subdivision regulations is provided as an attachment hereto. The subject project will conform to all City standards except those which are in conflict with assuring a high quality and cohesive aesthetic design or those that simply are not appropriate for the proposed use or may be unique to the site circumstances.
- viii. There are only three existing streets or roadways that impact or serve the subject property (no new streets or roadways are anticipated or provided for in the City Comprehensive Plan). The three existing streets / roadways are: I-35, a high speed freeway with service roads; Meadows Oak Drive, two lane existing road that will become an east – west thoroughfare (future ROW and expansion are anticipated); and Tower Ridge Trail, a north south collector that has recently been expanded to its probable ultimate size of a collector.
- ix. The property elevation generally ranges from 623 feet to 591 feet and is a gently rolling site sloping to the center from both the east and west. The property drains to both the north and south providing for natural drainage both north and south. The site features a combination of Birome and Gasil fine sandy loams.
- x. The applicant has provided a topographic map.
- xi. It appears the drainage area is generally equally divided with half flowing to the north and half flowing to the south. None of the property is in a flood zone; final civil plans will detail drainage plans and detention details. The nearest floodplain is over 550 feet to the west and over 500 feet to the east.

- xii. Water is adjacent to the property along roadways both east and west of the property. Wastewater is also adjacent to the property. Both water and wastewater will be extended into the property to serve the buildings and provide fire flow protection per the subdivision ordinance; plans for said utilities will be part of the project civil design subject to City review.

- xiii. There are no proposed residential densities on the property with all proposed development being commercial / business use in conformance with the proposed PD regulations. The structures on the site will total approximately 104,750 Square feet of space on approximately 13.5 acres; this will represent a floor to area ratio of about 0.178, a very low ratio. However, there will be several acres of paved parking for vehicle inventory.

- xiv. The project will be divided into two phases as illustrated on the Concept / Site Plan and Phasing Plan. Generally phase I will include the three sales venues and the initial phase of the service center as illustrated below on the PD Concept Design Map. Expansion of the Service Center and the addition of the Collision Center along with associated parking will be at a later date. The size of all facilities is detailed in the PD Zoning Ordinance.

PD CONCEPT DESIGN MAP



CERTIFICATE OF APPROVAL – PD DESIGN MAP

Approved

 Mayor, City of Corinth, Texas

 Date

The undersigned, the City Secretary of the City of Corinth, Texas, hereby certifies that the foregoing Design Map for Huffines Kia Subaru Master Planned Development was submitted to the City Council on the _ day of March, 2016 and the Council by formal action, then and there accepted the PD Design Map, and said Council further authorizes the Mayor to note the acceptance thereof by signing his/her name as hereinabove subscribed.

Witness by hand this ___ day of April , 2016.

 City Secretary
 City of Corinth, Texas

 Date

EXHIBIT "C"
LAND USE REGULATIONS

SECTION 1: REGULATIONS

A. Purpose

The regulations set forth in this Exhibit provide development standards for Commercial designations and use of the Huffines Automotive Dealerships. The Planned Development (PD) District is identified by metes and bounds on Exhibit A and is depicted on Exhibit B. Every use not authorized herein is expressly prohibited in this Planned Development (PD) District.

B. Base Districts

The "MX-C" Mixed – Use Commercial District, the regulations of the Corinth Unified Development Code, per the City of Corinth Unified Development Code, Ordinance No. 13-05-02-08, shall apply except as modified herein.

SECTION 2: USES AND AREA REGULATIONS FOR Planned Development MX-C (MX-C) District plus the uses detailed below:

A. Purpose

This district is intended to provide for development of two new automobile dealerships, one previously-owned dealership (which will serve as the previously owned component for both new Automobile dealerships), two connected service centers and parts inventory and sales (part of which will be a phase II component), and a collision center (which will also be a phase II component).

B. Permitted Uses and Use Regulations

In the Planned Development (PD) District for Mixed Use-Commercial Uses, no building or land shall be used and no building shall be hereafter erected, reconstructed, enlarged or converted, unless permitted by the Mixed-Use Commercial (MX-C) District regulations of the Unified Development Code except as otherwise included in this PD Ordinance.

The Permitted Uses in the MX-C, Mixed-Use Commercial District as listed in Subsection 2.07 of the Unified Development Code, shall be permitted uses and shall also include the following use:

1. Automobile Sales and Service
2. Automobile Body Shop
3. Above Ground Fuel Tank

The following uses are permitted under MX-C zoning, but shall not be permitted within this Planned Development District by Specific Use Permit or any other means:

1. Assisted Living/Nursing Home
2. Dwelling, Single Family (detached)
3. Dwelling, Single Family (attached – duplex)
4. Dwelling, Single Family (attached – townhouse)
5. Dwelling, Multi-Family
6. Modular (Industrialized) Home
7. Retirement Housing
8. Studio Residence

C. Development Standards

The Development Standards for this PD are the development standards for MX-C Mixed-Use Commercial, Section 2.05.02 of the City's Unified Development Code except as otherwise stated herein.

1. UDC 2.07.07 **Accessory Buildings and Uses** shall apply.
2. UDC 2.08.05 **Nonresidential Dimensional Regulations Chart** shall apply.
3. UDC 2.09.01 **Landscape Regulations** shall apply except for the following:
 - a. Modifications to the Phase I landscape Plan that are consistent with the original intent of the plan and maintain the use of City approved plant materials may be approved by the Director of Planning or designee. The Phase II landscape plan shall be reviewed and approved by the City Planning Director or designee prior to issuance of a building permit for Phase II of the project.
4. UDC 2.09.02 **Tree Preservation Regulations** shall apply except:
 - a. The "heavily treed lot provision" of the UDC shall be modified for this PD to apply to those portions of the site that have an aggregate of 4 acres or more and are delineated on the tree survey at the time of development (grading) permit or building permit.
5. UDC 2.09.03 **Vehicle Parking Regulations** shall apply except for the following clarifications and supplemental regulations:
 - a. The provisions of UDC 2.09.03 shall apply for customer and employee parking and shall measure 10'x20". Employee parking shall be required for the number of employees on the site without regard for the square footage of the work area. Customer parking shall be calculated based on building square footage provided for customer shopping, financing and automobile transaction, waiting, and

related public spaces. Areas for service, parts inventory and employee work stations may be excluded from the calculation of customer parking.

- b. Parking stalls intended for display, maintenance storage, and inventory storage purposes may measure 10'x18'.
- c. The parking ratio for customer accessible areas (including sales and lounge space) shall be 1 space per 350 square feet of floor area, as shown in the charts on the site plan associated with this PD.

6. UDC 2.09.04 **Building Façade Material Standards** shall apply and include the following percentages:

Kia Dealership

| Exterior Materials | Total % all sides | North | | East | | South | | West | | Total Sq.Ft. all |
|--|-------------------|--------------|-------|--------------|--------------|--|-------|--------------|-------|------------------|
| | | Sq.Ft. | % | Sq.Ft. | % | Sq.Ft. | % | Sq.Ft. | % | |
| Glazing | 28.8% | 766 | 22.2% | 1,414 | 41.0% | 966 | 28.0% | 300 | 8.7% | 3,446 |
| Metal** | 5.3% | 112 | 17.6% | 180 | 28.3% | 126 | 19.8% | 217 | 34.2% | 635 |
| Stucco | 25.9% | 490 | 15.8% | 873 | 28.2% | 680 | 21.9% | 1,057 | 34.1% | 3,100 |
| Stone | 1.4% | 72 | 43.1% | 15 | 9.0% | 72 | 43.1% | 8 | 4.8% | 167 |
| Arch. CMU | 25.9% | 871 | 28.1% | 70 | 2.3% | 417 | 13.5% | 1,740 | 56.2% | 3,098 |
| Tile | 12.6% | 404 | 26.8% | 738 | 48.9% | 354 | 23.5% | 12 | 0.8% | 1,508 |
| Total | 100.0% | 2,715 | | 3,290 | | 2,615 | | 3,334 | | 11,954 |
| Total Square Footage of Exterior Wall Area* | | | | | 8,508 | *excludes fenestration and doors | | | | |
| Total Square Footage of Masonry Wall Area | | | | | 7,873 | | | | | |
| Total Percentage of Class 1 & 2 Masonry | | | | | 92.5% | ** most metals have masonry backing | | | | |

Subaru Dealership

| Exterior Materials | Total % all sides | North | | East | | South | | West | | Total Sq.Ft. all |
|--|-------------------|--------------|-------|--------------|--------------|--|-------|--------------|--------|------------------|
| | | Sq.Ft. | % | Sq.Ft. | % | Sq.Ft. | % | Sq.Ft. | % | |
| Glazing | 28.1% | 1,344 | 38.0% | 1,243 | 35.2% | 636 | 18.0% | 310 | 8.8% | 3,533 |
| Metal** | 7.1% | 190 | 21.4% | 428 | 48.3% | 120 | 13.5% | 149 | 16.8% | 887 |
| Stucco | 2.4% | | | | | | | 302 | 100.0% | 302 |
| Stone | 9.4% | 235 | 19.9% | 490 | 41.5% | 386 | 32.7% | 70 | 5.9% | 1,181 |
| Arch. CMU | 33.2% | 916 | 21.9% | | | 916 | 21.9% | 2,343 | 56.1% | 4,175 |
| Tile | 19.8% | 835 | 33.6% | 932 | 37.5% | 654 | 26.3% | 62 | 2.5% | 2,483 |
| Total | 100.0% | 3,520 | | 3,093 | | 2,712 | | 3,236 | | 12,561 |
| Total Square Footage of Exterior Wall Area* | | | | | 9,028 | *excludes fenestration and doors | | | | |
| Total Square Footage of Masonry Wall Area | | | | | 8,141 | | | | | |
| Total Percentage of Class 1 & 2 Masonry | | | | | 90.2% | ** most metals have masonry backing | | | | |

Pre-Owned Dealership

| Exterior Materials | Total % all sides | North | | East | | South | | West | | Total Sq.Ft. all |
|--|-------------------|--------------|-------|--------------|-------|--|-------|--------------|-------|------------------|
| | | Sq.Ft. | % | Sq.Ft. | % | Sq.Ft. | % | Sq.Ft. | % | |
| Glazing | 25.2% | 484 | 24.1% | 808 | 40.3% | 484 | 24.1% | 230 | 11.5% | 2,006 |
| Metal** | 6.2% | 140 | 28.3% | 160 | 32.3% | 140 | 28.3% | 55 | 11.1% | 495 |
| Stucco | 13.9% | 110 | 10.0% | | | 110 | 10.0% | 884 | 80.1% | 1,104 |
| Stone | 9.6% | 234 | 30.5% | 180 | 23.4% | 234 | 30.5% | 120 | 15.6% | 768 |
| Arch. CMU | 20.9% | 578 | 34.7% | | | 578 | 34.7% | 512 | 30.7% | 1,668 |
| Tile | 24.2% | 615 | 31.9% | 689 | 35.8% | 622 | 32.3% | | | 1,926 |
| Total | 100.0% | 2,161 | | 1,837 | | 2,168 | | 1,801 | | 7,967 |
| Total Square Footage of Exterior Wall Area* | | | | 5,961 | | *excludes fenestration and doors | | | | |
| Total Square Footage of Masonry Wall Area | | | | 5,466 | | | | | | |
| Total Percentage of Class 1 & 2 Masonry | | | | 91.7% | | ** most metals have masonry backing | | | | |

Service Center

| Exterior Materials | Total % all sides | North | | East | | South | | West | | Total Sq.Ft. all |
|--|-------------------|--------------|-------|--------------|-------|--|-------|--------------|-------|------------------|
| | | Sq.Ft. | % | Sq.Ft. | % | Sq.Ft. | % | Sq.Ft. | % | |
| Glazing | 7.7% | 744 | 51.2% | 552 | 38.0% | | | 156 | 10.7% | 1,452 |
| Metal** | 9.0% | 498 | 29.5% | 525 | 31.1% | 214 | 12.7% | 450 | 26.7% | 1,687 |
| Stone | 3.2% | 42 | 7.0% | 338 | 56.4% | 147 | 24.5% | 72 | 12.0% | 599 |
| Concrete | 80.1% | 2,940 | 19.6% | 3,393 | 22.6% | 4,927 | 32.8% | 3,752 | 25.0% | 15,012 |
| Total | 100.0% | 4,224 | | 4,808 | | 5,288 | | 4,430 | | 18,750 |
| Total Square Footage of Exterior Wall Area* | | | | 17,298 | | *excludes fenestration and doors | | | | |
| Total Square Footage of Masonry Wall Area | | | | 15,611 | | | | | | |
| Total Percentage of Class 1 & 2 Masonry | | | | 90.2% | | ** most metals have masonry backing | | | | |

Collision Center

| Exterior Materials | Total % all sides | North | | East | | South | | West | | Total Sq.Ft. all |
|--|-------------------|--------------|-------|--------------|-------|--|-------|--------------|-------|------------------|
| | | Sq.Ft. | % | Sq.Ft. | % | Sq.Ft. | % | Sq.Ft. | % | |
| Glazing | 5.4% | 220 | 33.3% | | | | | 440 | 66.7% | 660 |
| Metal** | 6.5% | 229 | 29.0% | 86 | 10.9% | 140 | 17.7% | 335 | 42.4% | 790 |
| Stone | 5.3% | 120 | 18.7% | 180 | 28.0% | 120 | 18.7% | 222 | 34.6% | 642 |
| Concrete | 82.8% | 2,641 | 26.2% | 2,462 | 24.5% | 2,854 | 28.3% | 2,112 | 21.0% | 10,069 |
| Total | 100.0% | 3,210 | | 2,728 | | 3,114 | | 3,109 | | 12,161 |
| Total Square Footage of Exterior Wall Area* | | | | 11,501 | | *excludes fenestration and doors | | | | |
| Total Square Footage of Masonry Wall Area | | | | 10,711 | | | | | | |
| Total Percentage of Class 1 & 2 Masonry | | | | 93.1% | | ** most metals have masonry backing | | | | |

7. UDC 2.09.05 Residential Adjacency Standards shall apply except:
 - a. UDC 2.09.05.1 Building materials will be permitted as shown on attached elevations.
 - b. UDC 2.09.05.2 No building, including buildings with a footprint of six thousand (6,000) square feet or less (as provided for in the UDC 2.09.05), shall be required

to feature a pitched roof so as to maintain continuity of architectural design across the entire site.

8. UDC 2.09.06 **Nonresidential Architectural Standards** shall apply.
9. UDC 2.09.07 **Lighting and Glare Regulations** shall be subject to the following exceptions and supplemental provisions:
 - a. LED light sources (luminaires) are accepted as a light source.
 - b. UDC 2.09.07 C.3-4 (required turning off of Parking Luminaires and Building Luminaires). It is provided that site lighting is specifically considered to serve a prime security purpose across a large area of vehicle inventory, storage, and display and is critical for security. To achieve the security objective, the site lighting shall be permitted to maintain the design lighting standards (maximum of 19.9 fc, minimum of 4.2 fc, and 10.22 fc average along dealership frontage; maximum 12.6 fc, minimum of 0.0 fc, and 4.18 fc average on the balance of the project), except as follows:
 - c. UDC 2.09.07 G.1 (Intensity of Illumination from property to other adjacent properties). This provision shall apply to properties adjacent to the subject site based on the current zoning of the parcels (none are residential zoning). All said properties are projected to be commercial districts; thus maximum fc between properties shall be 3.0 fc for commercial district adjacency except for the above noted residential adjacencies until such time as the residual adjacency is eliminated.
 - d. Additional lighting detail:
 - i. All light sources (pole or building mounted) shall be shielded so as to not allow light to be directed off the property.
 - ii. Free standing light poles shall be a maximum of 22 feet tall, not counting the concrete base; standards may be mounted on concrete pedestals not to exceed 30 inches in height.
10. UDC 4.01 **Sign Regulations** shall apply except as follows:
 - a. UDC 401.15 C - Monument Sign provisions shall be adjusted as follows to reflect the extended distance from the proposed I-35 access roads to the property line and to encourage continuity with site landscaping:
 - i. There shall be a "0" setback from property line to the nearest portion of a monument sign (either base or sign face).
 - ii. Sign area and dimensions are as follows:
 - (a) Length (horizontal) of sign face shall be a maximum of 15'.
 - (b) Height (vertical) – Maximum vertical dimension of sign face shall be 7'.

be restricted to the maximum total building name / identification square footage calculation as provided in the UDC for the “front façade”. Such “front façade” square footage calculation may be placed or distributed on multiple single plane facades as shown on the building elevations attached herewith. Additionally, the maximum letter size permitted shall be 36” tall.

- ii. Service, Parts, and Other Building (such as service / collision building) Identification Needs shall be allowed one primary name / identification sign per building on either the building frontage facing toward the I-35 highway or facing west. Primary building names shall have a maximum of 36” high letters. Secondary building identification signs may be allowed on other building faces to provide building names for deliveries, etc. Secondary names and signage directing customers to service drive or entrance drives shall have a maximum of 24” high letters.
- iii. Sign placement on building – there shall be no restriction on the placement of attached signs for building name or identification on the structure so long as the aggregate width of signage on the front façade of a building does not exceed 50% of the façade width. Customer directions for service entrance shall not be subject to this provision. Non-frontage elevations shall not be permitted to have signage exceeding 40% of the non-frontage façade width.

d. Sign permits will require permitting aside from the building permit.

11. UDC 4.02 **Fence and Screening Regulations** shall apply except as follows:

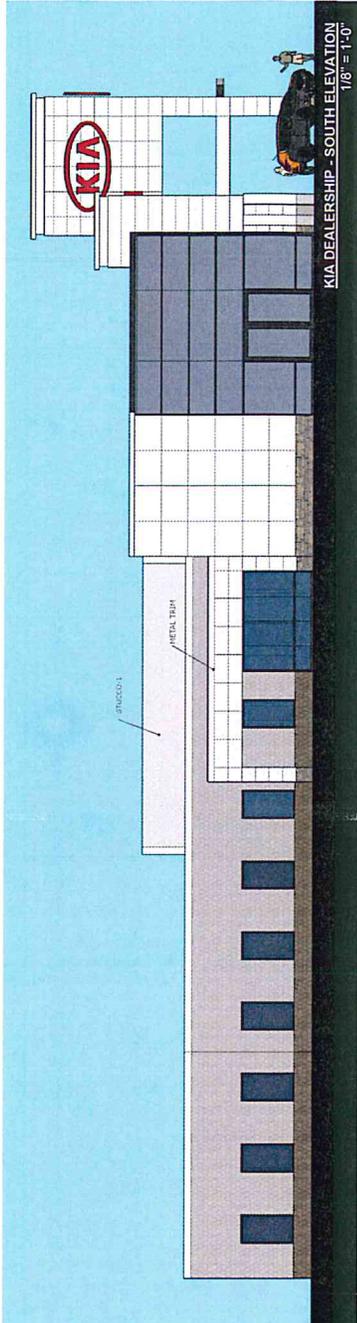
- a. A pipe rail not to exceed 30” in height shall be allowed around any portion of the site to supplement security.
- b. In order to provide for screening and security through intensive existing trees and vegetation, metal decorative fencing (tubular steel, wrought iron, and other decorative steel fencing, but not including chain link) shall be allowed around the perimeter of the site as illustrated on the Concept Plan. Said decorative metal fencing shall be no less than 6 feet and no more than 8 feet in height. Vegetative screening may be installed along with said fencing in areas where existing trees are not present to provide reasonable screening. If subsequently needed to provide needed screening for the internal areas used for storage of vehicles that are dismantled, wrecked, or in the process of being restored, a form of permitted solid or opaque fencing other than chain link must be approved by the Director of Planning.
- c. All decorative or security fencing, security pipe rail, and gates shall be shown on the site plan.
- d. All exterior fencing shown on the site plan shall be located on the property line, except where such fencing may deviate to preserve native trees.

- e. In non-vegetative areas, tubular metal fencing shall be used (generally in areas near frontage roads lacking existing trees and vegetation, but also as depicted on the Site Plan).
 - f. Concrete or masonry fencing shall not be required along the boundaries having a single family residential structure if such property is not zoned for single family use or if said property is expected to be redeveloped to non-residential uses.
 - g. Concrete (must be textured) or solid masonry fencing shall be required to screen the collision center if said facility is located within 100 feet of any property line across from residential and shall also be required along the site's frontage on Tower Ridge Drive.
 - h. All proposed fencing will require a fencing permit prior to construction.
12. Outside Display – outside vehicle (product) display is defined as positioning vehicles in locations intended to promote the brands, connect with consumer interest, highlight vehicle features, and show popular colors in a manner that rotates a variety of vehicles in locations as described below:
- a. Vehicle Parking Areas and Building Areas – These are locations behind the landscape setback where vehicles are parked for viewing in rows or in spots near or around the sales buildings that are reserved for vehicle display as indicated on the concept plan and / or illustrated in the provided pictorial graphics showing elevations and building perspectives.
 - b. Vehicle Display in Portion of Front Landscape Setback – a maximum of 10 vehicles (2 per side of entry drive) may be displayed in the westernmost 25' of the 30' landscape setback ("landscape setback vehicle locations") along the eastern North – South property line of the site (frontage adjacent to I-35) due to the unusually extreme distance between the east property line and the I-35 service road. The provision is subject to the following landscape vehicle display location criteria:
 - i. Be shown on the site plan or landscape plan.
 - ii. Blend with the landscape plan and bio-detention basins, including any sloping alignment of the vehicle (no mechanical or steel structure onto which vehicles are driven or placed); may include natural berms or blended stone bases onto which the vehicles are driven.
 - iii. Provide for a paved surface (including concrete, stamped concrete, or stone) that is designed to support the vehicle and preserve the integrity of the landscape.
 - iv. Vehicle Display in a portion of the Front Landscape Setback will terminate as a right when the Texas Highway Department widens or enlarges the highway (or service / access roads), so that the service / access roads are within an average

of 20' from the east property line of the subject property. Until that time, vehicles may remain on display after business hours and on weekends and holidays.

13. Project Phasing and Components Included in each phase:

- a. Phase I – shall include:
 - i. Two new vehicle dealership buildings / spaces and one pre-owned dealership building
 - ii. The initial service / maintenance / parts building for the site will be physically connected to the new vehicle dealership buildings / spaces.
 - iii. Parking required for each of the above facilities for customers, employees, and inventory
 - iv. 3 Paved ingress-egress locations from IH-35 service roads including the north-most ingress-egress location which is designated as a mutual access drive to serve the adjacent and subject properties. Note: it is understood that ingress-egress locations intersecting I-35 are subject to TXDOT approval.
 - v. Landscaping as illustrated on the landscape plan approved with this ordinance and the Concept / Site Plan; said landscape plan may be modified subject to modifications maintaining the basic design and changes to species of trees and other plant material being on the approved plant list and such modifications shall require staff approval.
 - vi. Three monument signs – one for each dealership
 - vii. Pipe rail surrounding the site with movable entry pipe rail provisions at entrances
 - viii. Fencing as detailed on the site plan, including all types of fencing around the entire perimeter of the property
 - ix. One compactor dumpster unit to serve the property and the required access drives to service said compactor.
 - x. As a matter of development sequence information, the electric pole relocation will be part of phase I as required for service regardless of the phase in which such distribution site is located.
- b. Phase II – shall include
 - i. A second phase of the service-maintenance-parts building
 - ii. A collision repair building
 - iii. Parking required for each of the above facilities for customers, employees, and inventory (vehicles being serviced)
 - iv. An additional ingress-egress location with paved fire lane including a City approved accessible locking device (if gated) to accommodate emergency access. This fire lane may be allowed as part of phase I if required by the Fire Marshall.



KIA DEALERSHIP - SOUTH ELEVATION
 1/8" = 1'-0"

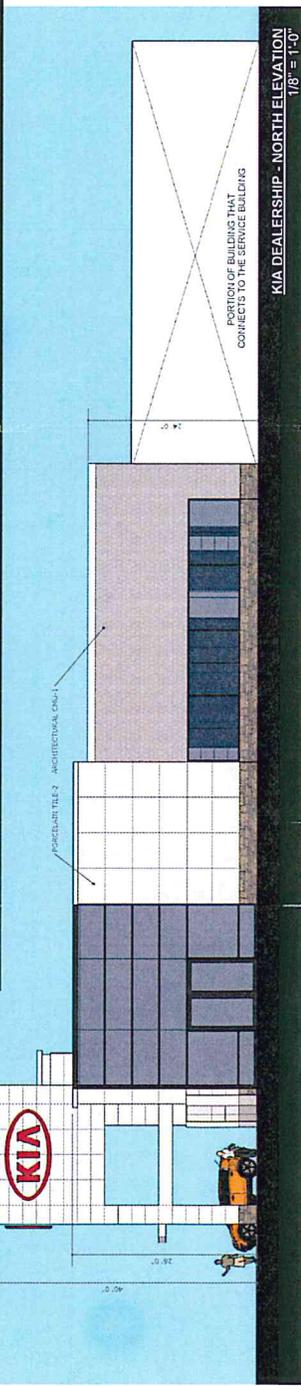
| Material | Qty | Unit | North | East | South | West | Total |
|--------------|---------------|-----------|---------------|---------------|----------|----------|---------------|
| Masonry | 51,115 | SF | 10,000 | 10,000 | 10,000 | 10,000 | 40,000 |
| Stucco | 2,726 | SF | 1,363 | 1,363 | 0 | 0 | 2,726 |
| Stone | 5,311 | SF | 2,656 | 2,655 | 0 | 0 | 5,311 |
| Tile | 18,184 | SF | 9,092 | 9,092 | 0 | 0 | 18,184 |
| Signage | 1 | SF | 1 | 1 | 0 | 0 | 2 |
| Other | 1,000 | SF | 500 | 500 | 0 | 0 | 1,000 |
| Total | 84,347 | SF | 42,173 | 42,173 | 0 | 0 | 84,346 |

Total Square Footage of Exterior Wall Area* 84,347
 *Excludes Foundation and Deck

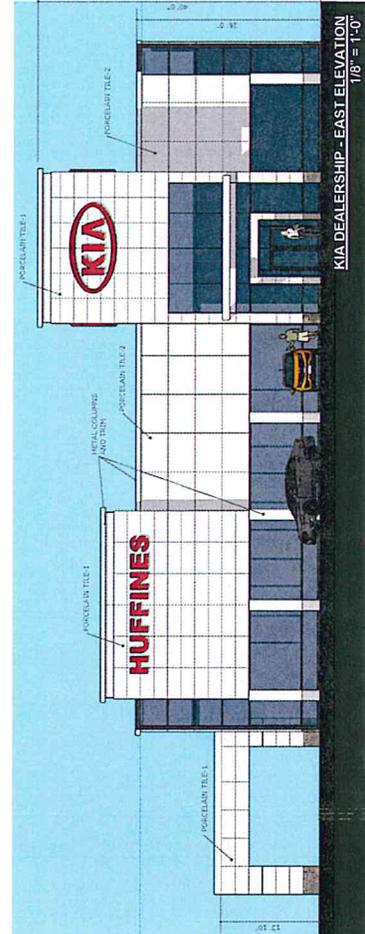
Exterior Surface Area of Glass 1 Masonry Wall Area 4,412
Total Masonry Surface Area of Glass 1 Masonry Wall Area 3,543
Total Percentage of Glass 1 Masonry 3.98%

Exterior Surface Area of Glass 2 Masonry Wall Area 1,000
Total Masonry Surface Area of Glass 2 Masonry Wall Area 1,000
Total Percentage of Glass 2 Masonry 1.19%

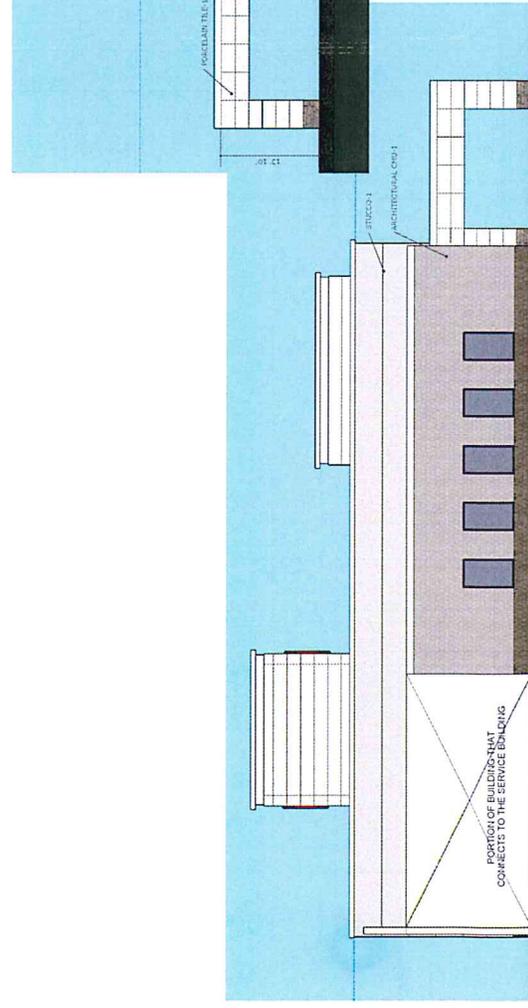
**** exact match to our industry practice**



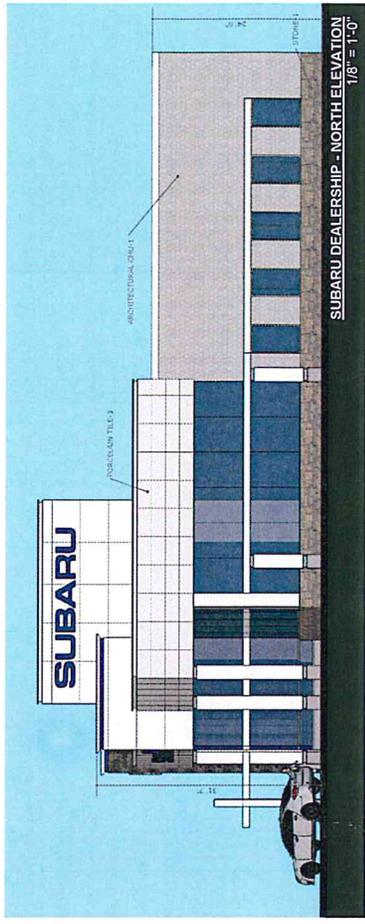
KIA DEALERSHIP - NORTH ELEVATION
 1/8" = 1'-0"



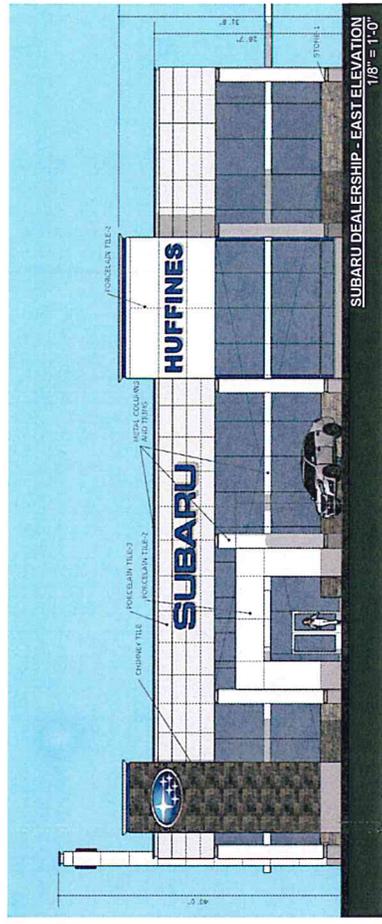
KIA DEALERSHIP - EAST ELEVATION
 1/8" = 1'-0"



KIA DEALERSHIP - WEST ELEVATION
 1/8" = 1'-0"

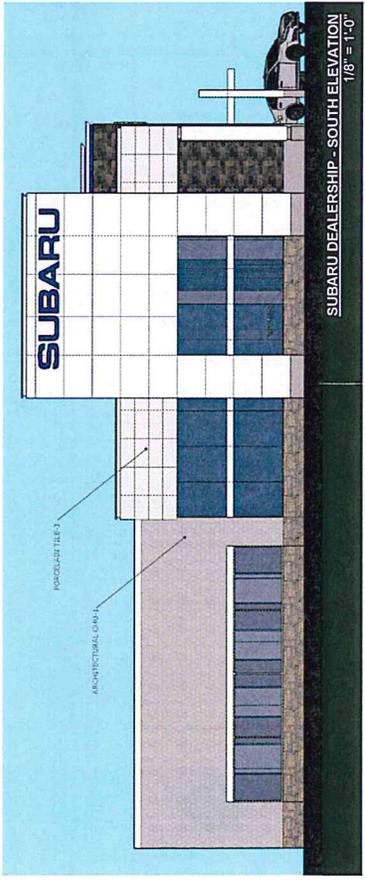


SUBARU DEALERSHIP - NORTH ELEVATION
 1/8" = 1'-0"

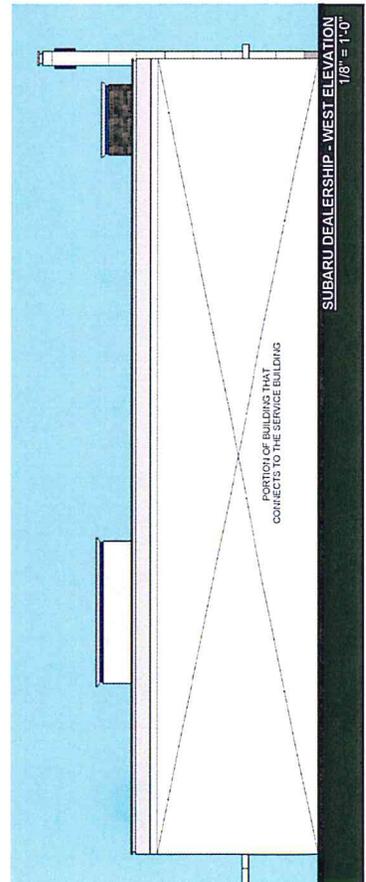


SUBARU DEALERSHIP - EAST ELEVATION
 1/8" = 1'-0"

| Material | Total Sq. Ft. | North | East | South | West | Total |
|----------------------|---------------|-------|------|-------|------|-------|
| Materials at 1/8\"/> | | | | | | |



SUBARU DEALERSHIP - SOUTH ELEVATION
 1/8" = 1'-0"



SUBARU DEALERSHIP - WEST ELEVATION
 1/8" = 1'-0"

PORTION OF BUILDING THAT
 CONNECTS TO THE SERVICE BUILDING

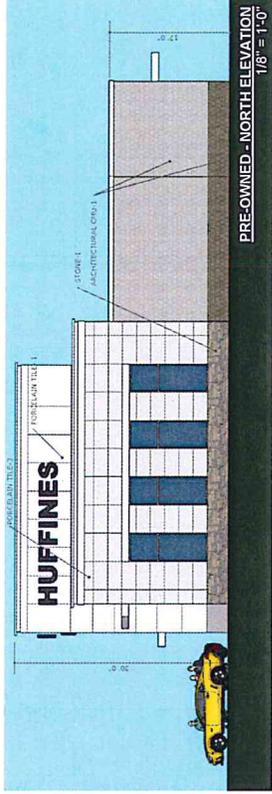
** most materials have masonry backing

PRE-OWNED

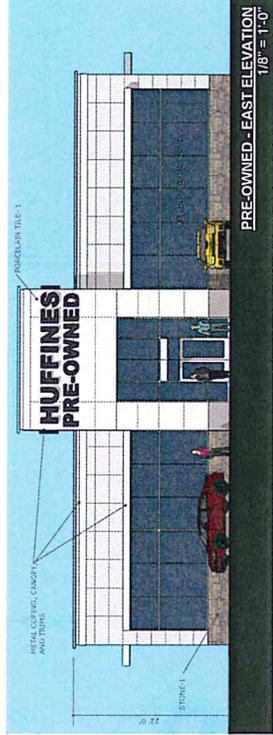
| Material | Total Sq. Ft. | North | South | East | West | Total |
|--|---------------|-------|--------------|-------|--------|--------|
| Brick | 17,760 | 1,780 | 13,976 | 800 | 67,760 | 83,316 |
| Stucco | 4,000 | 75 | 24,276 | 78 | 25,526 | 49,879 |
| Stone | 8,500 | 100 | 31,300 | 180 | 31,900 | 63,380 |
| Arch. COPG | 26,400 | 500 | 31,800 | 500 | 31,800 | 64,100 |
| Tile | 35,100 | 810 | 34,600 | 210 | 35,500 | 70,910 |
| Total Square Footage of Exterior Wall Area* | | | 17,240 | 3,480 | 1,400 | 62,120 |
| Total Square Footage of Class 1 Masonry Wall Area | | | 2,989 | | | 2,989 |
| Total Square Footage of Class 2 Masonry Wall Area | | | 2,770 | | | 2,770 |
| Total Square Footage of Class 3 Masonry Wall Area | | | 51,361 | | | 51,361 |
| Total Percentage of Class 1 & 2 Masonry | | | 94.6% | | | |

* Includes Entrances and Doors

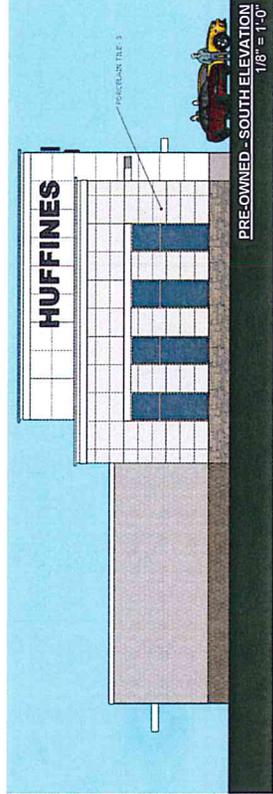
** most masonry has masonry back-up



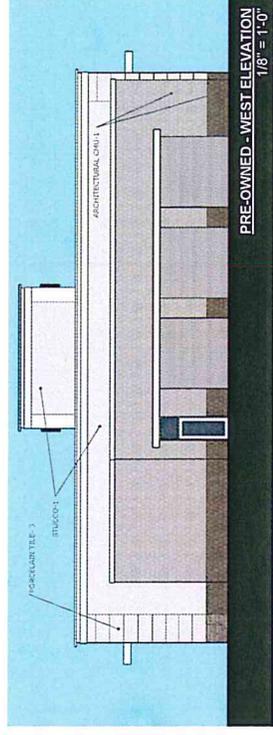
PRE-OWNED - NORTH ELEVATION
 1/8" = 1'-0"



PRE-OWNED - EAST ELEVATION
 1/8" = 1'-0"

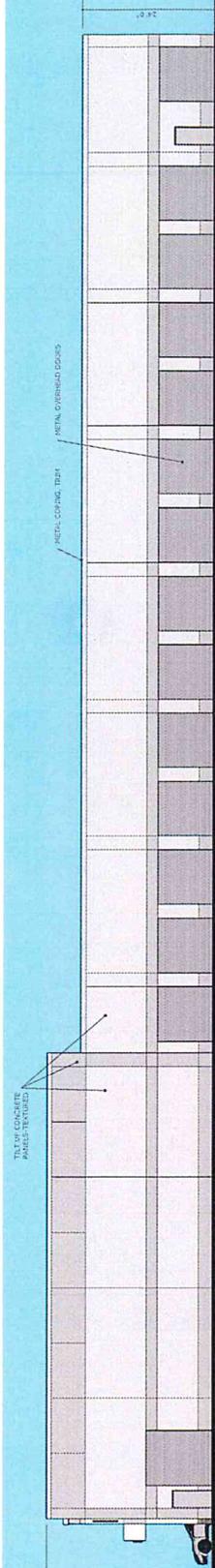


PRE-OWNED - SOUTH ELEVATION
 1/8" = 1'-0"

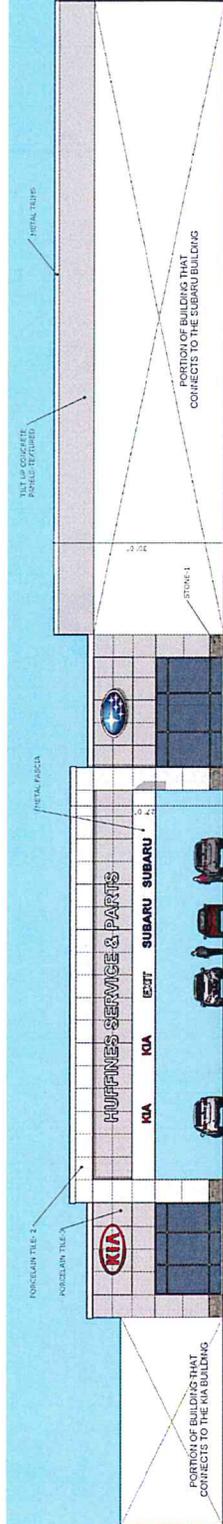


PRE-OWNED - WEST ELEVATION
 1/8" = 1'-0"

SERVICE CENTER - NORTH ELEVATION
 1/10" = 1'-0"



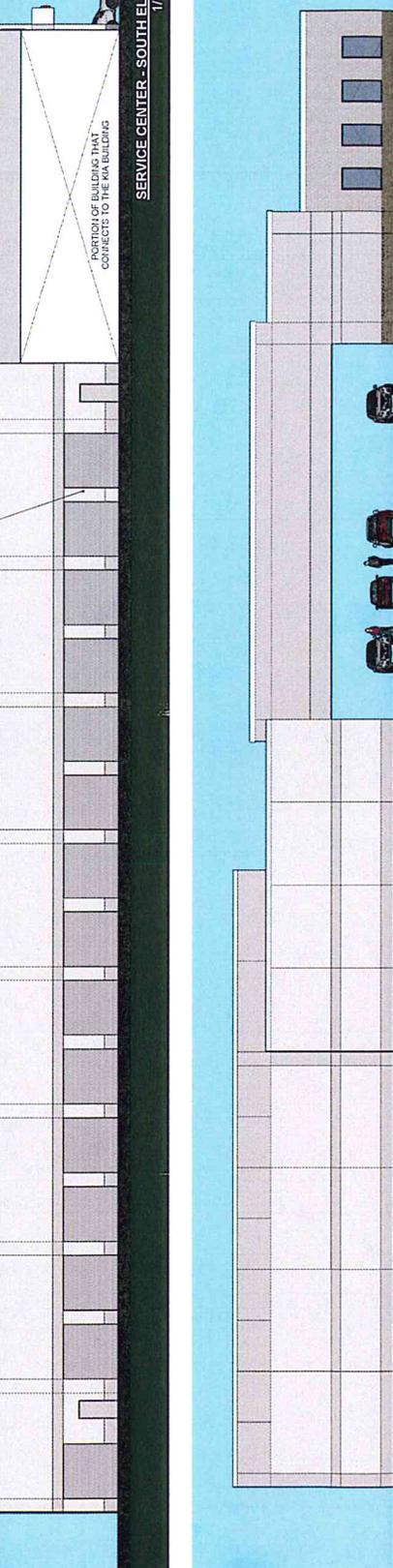
SERVICE CENTER - EAST ELEVATION
 1/10" = 1'-0"



SERVICE CENTER - SOUTH ELEVATION
 1/10" = 1'-0"

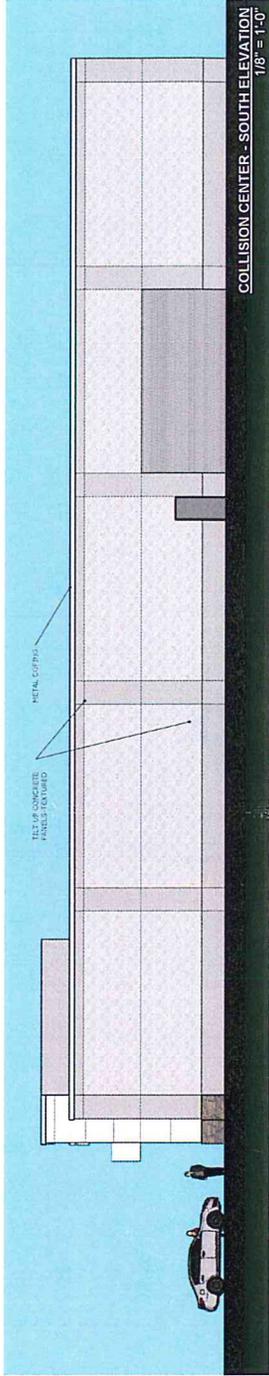


SERVICE CENTER - WEST ELEVATION
 1/10" = 1'-0"

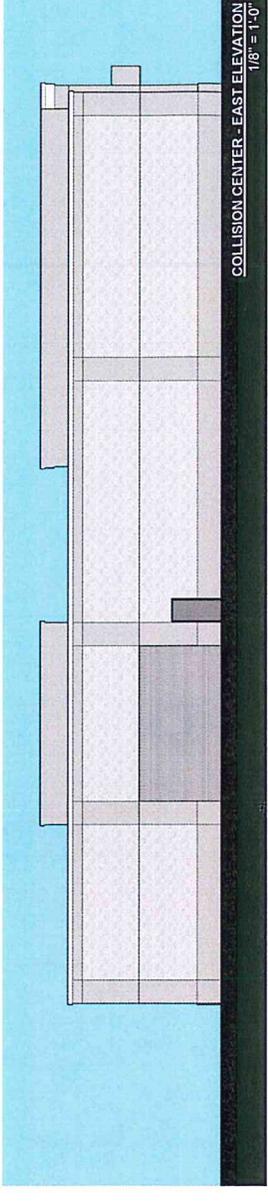
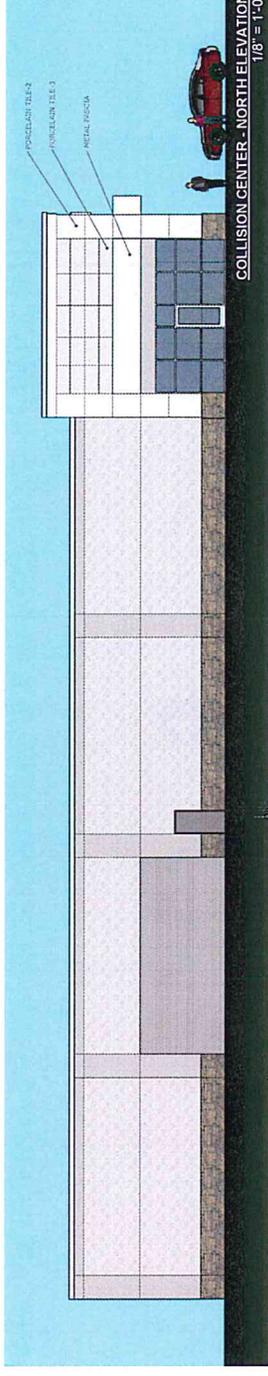
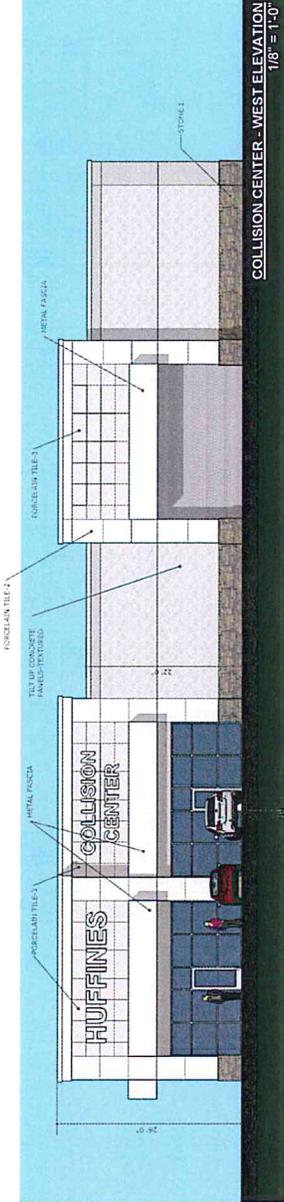


| SERVICE AND PARTS | | KIA | | SUBARU | | TOTAL | |
|-------------------|--------------|---------------|---------------|--------------|--------------|---------------|---------------|
| Area | % | Area | % | Area | % | Area | % |
| Garage | 3.1% | 4,312 | 40.6% | 1,041 | 10.0% | 5,353 | 50.6% |
| Office | 6.2% | 1,550 | 14.7% | 1,550 | 14.8% | 3,100 | 29.5% |
| Store | 2.0% | 1,425 | 13.6% | 1,425 | 13.7% | 2,850 | 27.2% |
| Other | 1.7% | 1,298 | 12.3% | 1,298 | 12.4% | 2,596 | 24.6% |
| Total | 13.0% | 13,317 | 126.0% | 8,362 | 79.6% | 21,679 | 205.6% |

Total Square Footage of Class 1 Warehouse With Drive: 10,900
 Total Square Footage of Class 2 Warehouse With Drive: 13,837
 Total Square Footage of Class 3 Warehouse: 11,297
 Total Warehouse Capacity: 36,034
 Total Capacity of Class 1 Warehouse: 11,676
 Total Capacity of Class 2 Warehouse: 14,358
 Total Capacity of Class 3 Warehouse: 14,600



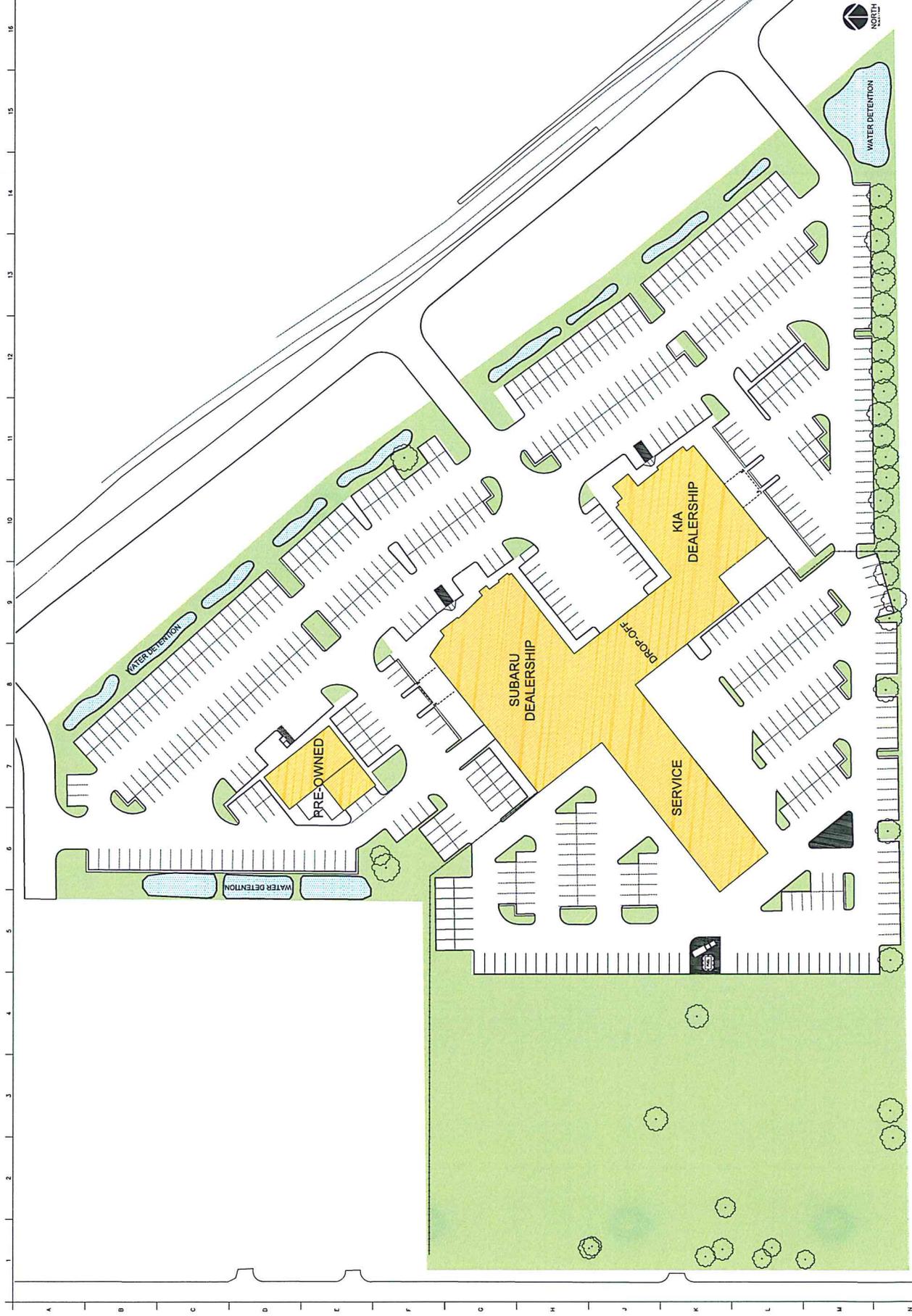
| Material | North | | East | | South | | West | | Total |
|---|-------|-------|-------|-----|-------|-------|-------|-------|--------|
| | Sq Ft | % | Sq Ft | % | Sq Ft | % | Sq Ft | % | |
| Cladding | 50.7% | 200 | 33.3% | 66 | 10.9% | 140 | 17.7% | 440 | 66.7% |
| Metal** | 60.7% | 239 | 29.0% | 66 | 10.9% | 140 | 17.7% | 331 | 42.4% |
| Stone | 74.3% | 445 | 46.6% | 186 | 18.6% | 120 | 12.6% | 221 | 23.0% |
| Concrete | 69.7% | 449 | 22.2% | 242 | 26.6% | 235 | 34.3% | 951 | 13.9% |
| Tile | 10.8% | 347 | 16.7% | 242 | 26.6% | 235 | 34.3% | 951 | 13.9% |
| Total | 80.6% | 3,090 | 27.2% | 272 | 31.1% | 3,114 | 3.10% | 3,107 | 11,041 |
| Total Square Footage of Exterior Wall Area** | | | | | | | | | |
| Total Square Footage of Class 1 Masonry Wall Area | | | | | | | | | |
| Total Square Footage of Class 2 Masonry Wall Area | | | | | | | | | |
| Total Percentage of Class 1 Masonry | | | | | | | | | |
| Total Percentage of Class 1 & 2 Masonry | | | | | | | | | |



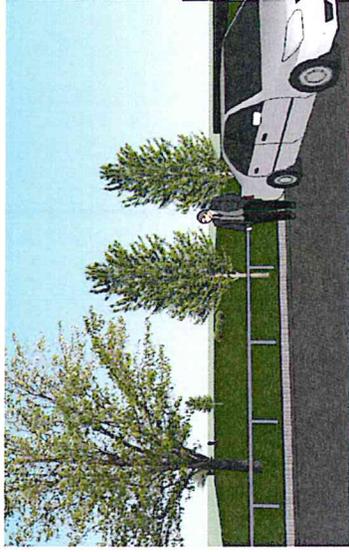
A NEW DEALERSHIP
FACILITY FOR:

| REVISIONS | |
|-----------|-------------|
| NO. | DATE / NOTE |
| | |
| | |
| | |
| | |

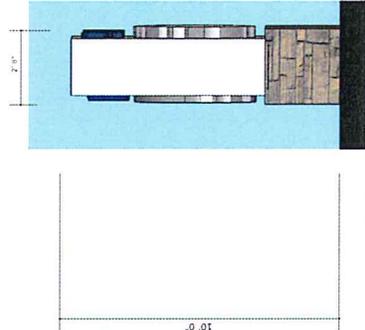
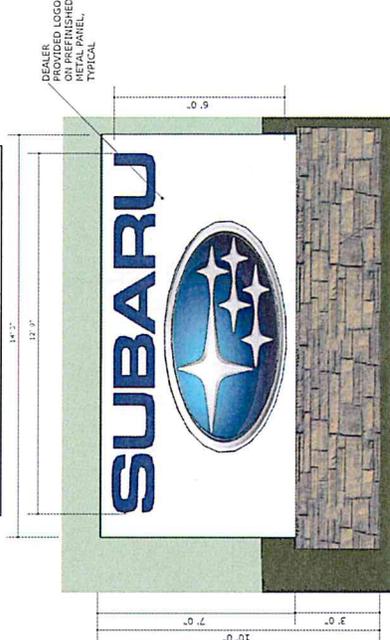
| | |
|--------------|------------|
| Drawn By: | ALM |
| Checked By: | TJC |
| Project No.: | 15-0105 |
| Date: | 03-18-2015 |



N1 SITE PLAN -PHASE I
SCALE: 1/8"=1'-0"



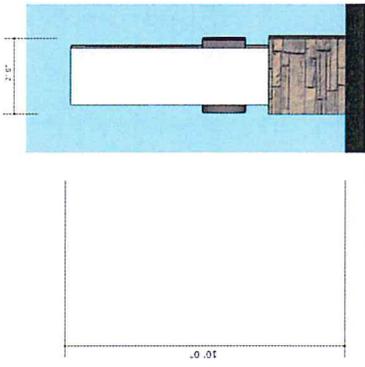
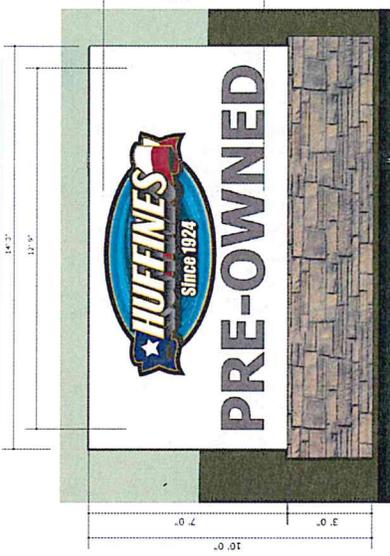
FENCE TYPE #3
2-0" HIGH RAIL



SUBARU MONUMENT SIGN
 1/2" = 1'-0"



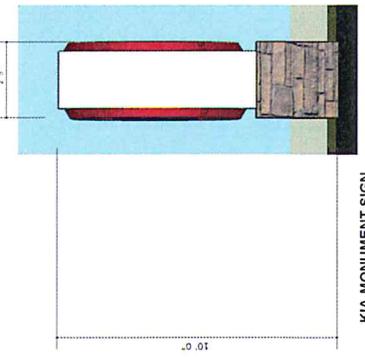
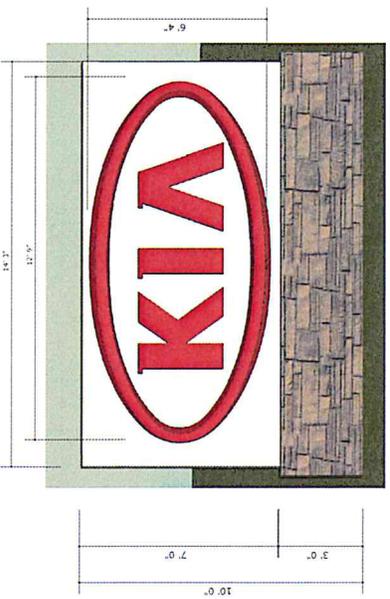
FENCE TYPE #2
6-0" HIGH MINIMUM MASONRY FENCE



PRE-OWNED MONUMENT SIGN
 1/2" = 1'-0"



FENCE TYPE #1
6-0" HIGH MINIMUM ORNAMENTAL METAL FENCE WITH PIPE RAIL



KIA MONUMENT SIGN
 1/2" = 1'-0"

Planned Development Zoning Provisions
Applicable to Zoning Regulations and Sign/Fencing Regulations

| Code Number | Required per UDC | Requested per PD |
|--|--|--|
| <i>Landscaping Regulations</i> | | |
| 2.09.01.6 | A 20-foot wide landscaped buffer shall be provided adjacent to existing residential or vacant land zoned for residential uses. | If the residential adjacency requirement is eliminated, the 20' buffer shall be replaced with a 5' minimum landscape buffer. |
| 2.09.01.5 a.-h. | Requirement for foundation plantings for buildings over 50,000 SF | While the Kia and Subaru dealerships (and portions of the service & parts building) are connected with a breezeway / canopy (no east or west wall or doors, we believe that they are two separate buildings, and as such do not meet the threshold for foundation planting. However, as a demonstration of good faith, we have reviewed our plans and can provide space on north & south sides of the KIA & Subaru buildings to add foundation plantings. This change has been made. |
| <i>Vehicle Parking Regulations</i> | | |
| 2.09.03.B.2 | All required parking spaces must be a minimum of 10'x20' | Parking stalls intended for display, maintenance storage, and inventory storage purposes may measure 10'x18' by the PD since they are not spaces that either the public or employees will be accessing multiple times per day. |
| 2.09.03.G | Parking ratios for uses not listed must be determined by Director of Planning | The applicant proposes that including a reasonable standard in the PD provisions is a positive regulatory provision and proposes defining the required customer parking ratio as 1 space per 350 square feet of sales and lounge areas (public space areas) |
| 2.09.03.G | Parking ratios for uses not listed must be determined by Director of Planning | Provides that required employee parking shall be based on the number of employees on site |
| <i>Residential Adjacency Standards</i> | | |
| 2.09.05.C.2 | All structures less than 6,000 SF must have a pitched roof | Structures less than 6,000 SF shall not be required to have a pitched roof thus providing the entire project a consistent design. |

| | | |
|---------------------------------------|--|---|
| 2.09.05.C.1 | The use of cement, standard concrete block, concrete tilt wall, stucco, and other similar masonry materials is not permitted | Materials may match what is shown on attached elevations. No standard "smooth" concrete block or similar material will be used. The maintenance building will be textured tilt wall which meets requirements. |
| <i>Lighting and Glare Regulations</i> | | |
| 2.09.07.C.3-4. | All parking luminaries must be turned off 1 hour after the business closes, and all building luminaires must be turned off after 10:00 PM, except for reasons of security | Lighting as shown on photometric plan shall be allowed to remain operational at all times for security reasons, but shall be reduced to levels agreed upon by the developer and the City |
| <i>Sign Regulations</i> | | |
| 4.01.15.2 | Maximum height for attached building sign letters is 24" for buildings less than 200' from the property line; 36" for buildings 251-300' from property line | Maximum height for attached building sign letters is 36" and 80" for logos |
| 4.01.15.4.a | A business shall be allowed 1 registered logo or name of the registered business per street frontage | Attached signs may be located on up to three sides of each building; however, total signage area must be less than or equal to the square footage allowed per UDC (10% of front façade area) |
| 4.01.15.B.5.a | Width of an attached sign may not exceed the middle 50% of the width of any lease space | Attached signs may be placed anywhere on the façade as long as the width of the sign does not exceed 50% of the width of the front façade, and 40% of all other facades |
| 4.01.15.E.2 | Minimum height of 6" for letters and logos | No limit on letter size other than what can fit on the maximum sign face area |
| 4.01.15.E.3 | Maximum monument sign structure height is 8 feet, but a height of 15 feet may be approved by the Director of Planning if the sign has a minimum of four performance based enhancements | Maximum sign height is 7 feet, maximum sign width is 15 feet. A 3' tall base shall not be considered part of the height requirement if no commercial message or logo is included |
| 4.01.15.E.4.a | Maximum sign area per face is 80 SF | Maximum sign area per face is 85 SF; measured as maximum area encompassed by vertical & horizontal limits of names, logos, & symbols within the sign face. |
| 4.01.15.E.5.a | Only one monument sign shall be permitted per street frontage per site | Each new or previously owned dealership may have 1 monument sign along I-35E as they would by right if platted separately |
| 4.01.15.E.5.c | 500' minimum distance between monument signs | 250' minimum distance between monument signs |

| | | |
|--|---|--|
| 4.01.15.E.6 | 15' minimum setback from any property line | No minimum setback from property lines due to distance from freeway |
| <i>Fencing & Screening Regulations</i> | | |
| 4.02.11.A | Masonry screening walls separating nonresidential uses and/or zoned areas from residential uses shall be at least six feet (6') in height | Masonry fencing shall be provided to screen Collision Center and along Tower Ridge Dr; does not seem reasonable for masonry screen to be required along boundaries having a single family residential structure if such property is non-conforming |

| Waivers to Subdivision Regulations | | |
|---|---|--|
| Code Number | Required Per UDC | Requested Per PD |
| 2.09.01.A.2.d | All landscaped areas shall be protected by a raised six inch curb | Parking spaces facing the front landscape buffer shall have individual curb stops to maximize drainage to the bio-swales along the frontage. |
| 2.09.02.B.4.a,c | A lot shall be considered "heavily treed" if it has tree canopy coverage of 50% or more; a heavily treed lot shall be allowed to reduce the amount of Protected Trees needing to be replaced by 50% | The "heavily treed lot provision" of the UDC shall be modified for this PD to apply to those portions of the site that have an aggregate 4 acres or more and are delineated on the tree survey at the time of development (grading) permit or building permit. |
| 2.09.02.D | At or before the Preliminary Plat or Site Plan review and prior to the removal of any trees, the Applicant shall submit a Tree Protection Plan, which shall graphically identify Protected Trees and identify those being preserved and those being removed. Notably, the Tree Protection Plan is submitted jointly with the Tree Survey or a previously approved Tree Survey, if development is occurring in stages or phases. | All grubbing, tree removal, and rough grading for the entire property may be completed in Phase I, after the required Tree Survey, Tree Protection Plan, and/or Tree Mitigation Plan have been submitted and approved by the appropriate authorities. |
| 3.04.01 | Preliminary plat must be submitted before construction plans are approved | Construction of the shared north driveway may take place before the property to the north is platted if an easement for the entire mutual access drive is provide whether by plat or by a separate easement document. In this case, the requirement for a preliminary plat to be approved for the property before the driveway may be constructed shall be waived. |

HUFFINES CORINTH DEALERSHIPS – BUILDING SIGNAGE

March 23, 2016

KIA

Allowed: 1 building sign at 312 SF max. (10% of 3,122 SF, Front Façade only)
Lettering and logo height limited to 24" (180 feet from property line)

Proposed: East Façade: 2 building signs totaling 131 SF
KIA logo: 6'-5" x 12'-9" = 82 SF
Huffines Name: 2'-6" x 19'-5" = 49 SF (30" high letters)
South Façade: 1 building sign totaling 82 SF
KIA logo: 6'-5" x 12'-9" = 82 SF
North Façade: 1 building sign totaling 82 SF
KIA logo: 6'-5" x 12'-9" = 82 SF

Total KIA Building Signage: 295 SF (less than max. of 312 SF)

SUBARU

Allowed: 1 building sign at 322 SF max. (10% of 3,226 SF, Front Façade only)
Lettering and logo height limited to 24" (180 feet from property line)

Proposed: East Façade: 3 building signs totaling 158 SF
Subaru logo: 4'-5" x 8'-2" = 36 SF
Subaru name: 3'-0" x 25'-8" = 77 SF
Huffines name: 2'-6" x 18'-0" = 45 SF (30" high letters)
South Façade: 1 building sign totaling 77 SF
Subaru name: 3'-0" x 25'-8" = 77 SF
North Façade: 1 building sign totaling 77 SF
Subaru name: 3'-0" x 25'-8" = 77 SF

Total Subaru Building Signage: 312 SF (less than max. of 322 SF)

PRE-OWNED

Allowed: 1 building sign at 194 SF max. (10% of 1,942 SF, Front Façade only)
Lettering and logo height limited to 24" (180 feet from property line)

Proposed: East Façade: 2 building signs totaling 128 SF
Huffines name: 2'-6" x 18'-8" = 47 SF (30" high letters)
Pre-Owned: 2'-0" x 18'-8" = 37 SF (24" high letters)
South Façade: 1 building sign totaling 49 SF
Huffines name: 2'-6" x 19'-5" = 49 SF (30" high letters)
North Façade: 1 building sign totaling 49 SF
Huffines name: 2'-6" x 19'-5" = 49 SF (30" high letters)

Total Pre-Owned Building Signage: 182 SF (less than max. of 194 SF)

SERVICE AND PARTS

Allowed: 1 building sign at 231 SF max. (10% of 2,316 SF Front Façade only)
Lettering and logo height limited to 36" (270 feet from property line)

Proposed: East Façade: 5 building signs totaling 231 SF
Huffines name: 2'-11" x 19'-0" = 56 SF (35" high letters)
KIA logo: 3'-6" x 7'-6" = 26 SF
Parts/Service: 2'-11" x 35'-0" = 102 SF (35" high letters)
Subaru logo: 3'-6" x 7'-6" = 26 SF
Lane signs: 1'-0" x 21'-0" = 21 SF (5 combined – 12" high letters)

Total Service/Parts Building Signage: 231 SF (equal to max. of 231 SF)

MONUMENT SIGNAGE

Allowed: 1 monument sign per site, with a maximum of 100 SF structure size and 80 SF sign size with a height not to exceed 8'. Sign is to be set back 15' from the property line, with 500' between signs.

Proposed: 3 monument signs along I-35 E access road, with a minimum setback of 0' from the property line. One sign per dealership. Each sign structure is a maximum of 100 SF, with a maximum of 85 SF for sign face, set on top of a 3' stone base (not included in the calculation of sign structure area). There will be 250' between the Pre-Owned sign and the Subaru Sign, and 450' between the Subaru sign and the Kia sign.

Subaru Sign: 6'-0" x 12'-9" = 77 SF

Kia Logo: 6'-4" x 12'-9" = 81 SF

Pre-Owned logo: 5'-8" x 12'-9" = 72 SF